



# City of Boyne City

Founded 1856

319 N. Lake Street

Boyne City, Michigan 49712  
[www.cityofboynecity.com](http://www.cityofboynecity.com)

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## AGENDA

### BOYNE CITY PLANNING COMMISSION

Monday May 20, 2019 5:00 p.m.  
Boyne City Hall



Scan QR code or go to  
[www.cityofboynecity.com](http://www.cityofboynecity.com)  
click on Boards & Commissions for complete  
agenda packets & minutes for each board

1. Call to Order
2. Roll Call - Excused Absences
3. Consent Agenda

*The purpose of the consent agenda is to expedite business by grouping non-controversial items together to be acted upon by one Commission motion without discussion. Any member of the Commission, staff, or the public may ask that any item(s) on the consent agenda be removed to be addressed immediately following action on the remaining consent agenda items. Such requests will be respected.*

Approval of minutes from the April 15, 2019 Boyne City Planning Commission meeting.

4. Hearing Citizens Present (*Non-Agenda Items*)
5. Reports of Officers, Boards, Standing Committees
6. Unfinished Business
7. New Business
  - A. Development Plan Review Boyne City Surgery Center
  - B. Development Plan Review Lofts on Lake Street
  - C. Preliminary Review Lake View Village
8. Staff Report
9. Good of the Order
10. Adjournment – Next Meeting June 17, 2019

*Individuals with disabilities requiring auxiliary aids or services in order to participate in municipal meetings may contact Boyne City Hall for assistance: Cindy Grice, City Clerk/Treasurer, 319 North Lake Street, Boyne City, MI 49712; phone (231) 582-0334*

**Meeting of  
April 15, 2019**

Record of the proceedings of the Boyne City Planning Commission meeting held at Boyne City Hall, 319 North Lake Street, on Monday April 15, 2019 at 5:00 pm.

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**Call to Order**

Chair Place called the meeting to order at 5:00 p.m.

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**Roll Call**

Present: Ken Allen, Jason Biskner, George Ellwanger, Tom Neidhamer, Aaron Place, Jeff Ross and Joe St. Dennis  
Absent: Jim Kozlowski  
Vacancy: One

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**Excused Absences**

**\*\*Motion**

**2019-4-15-02**

**St Dennis moved, Allen seconded, PASSED UNANIMOUSLY**, a motion to excuse the absence of Kozlowski

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**Meeting Attendance**

City Officials/Staff: Planning and Zoning Administrator Scott McPherson, City Manager Michael Cain, Main Street Director Kelsie King-Duff, Commissioners Hugh Conklin and Ron Grunch and Recording Secretary Pat Haver  
Public Present: Fifty One

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**Consent Agenda**

**\*\*Motion**

**2019-4-15-03**

**Allen moved, Ellwanger seconded, PASSED UNANIMOUSLY**, a motion to approve the consent agenda, the Planning Commission minutes from March 18, 2019 as presented.

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**Citizen comments on  
Non-Agenda Items**

**Lesley Pritchard: 361 Wildwood Ridge Walloon Lake** – Passed a handout to the Commissioners (received and filed) and read comments from it. She is requesting that the Planning Commission pass a motion to repeal the recent height/story amendment and make a recommendation to the City Commission to do the same.

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**Reports of Officers,  
Boards and Standing  
Committees**

None

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**Unfinished Business**

**Alley Vacation Request  
from Richard Wright**

**Richard Wright 920 Edmunds St. applicant** – The request is to vacate 198' portion of the alley between Edmund and Division St.; 132' west of Hull St. This alley is undeveloped and is not maintained by the city, there are no city utilities located in the area, and a 66' westerly portion of the alley along with Houghtlin street had previously been vacated. Neighboring property owners to the request did sign a petition in favor of the closure.

**McPherson** – Also reviewed the request and indicated that there were no plans or future need from the city for any infrastructure to access this portion of the alleyway.

**St. Dennis** – Will this make any of the adjacent properties landlocked?

**McPherson** – No

**Neidhamer** – Why is only a portion being closed and not the entire length?

**McPherson** – The landowner on the corner of Hull and Edmund St. does not desire to have that portion closed, and historically, other alleys in the city have partially been closed; as a portion of this one was previously; so it is not unusual to have a partial request.

**\*\*Motion**

With no further board discussion **motion by Ross, seconded by Ellwanger** to recommend to the City Commission, closure of the alley as presented; 198' in length, between Edmund and Division Street beginning 132' west of Hull St.

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Roll Call:  
2019-4-15 6A  
Ayes: Allen, Biskner, Ellwanger, Neidhamer, Place, Ross and St. Dennis  
Nays: None  
Absent: Kozlowski  
Abstain: None  
Vacancy: One  
*Motion Carries*

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**Recommendation for  
Planning Commission  
Appointment**

Planning Director McPherson reviewed his staff report included in the agenda packet. There is a vacancy on this commission with a term ending May 31, 2021. There were 4 submitted applications for the position, and all of them were invited to the meeting to meet and speak with the board. Each applicant was given an opportunity to introduce themselves, and to give a history of their background, the reasons they wish to serve and other community or civic service activities. Three applicants were in attendance, Adam Graef, Skylar MacNaughton, and Rose Newton all spoke to the board. After a question and answer session, a straw vote was taken with a **motion by Ross, seconded by Biskner, PASSED UNANIMOUSLY** to recommend Skylar MacNaughton and Rose Newton as possible candidates to the City Commission for their final decision and appointment.

**\*\*Motion**

**New Business**

**Preliminary Review  
Boyne City Surgery  
Center**

Planning Director McPherson reviewed his report included in the agenda packet. The surgical center is proposing a 7,200 sq ft outpatient facility located on lot #16 in the Business Park, on Moll Dr. north of M-75 across from Classic Instruments, they are here tonight for preliminary review and discussion

**Darren Graham, Project Manager: Gosling Czubak Engineering** - Current zoning is PID (Planned Industrial District) in Phase II of the park. The plan will be for minor, same day surgical procedures with no overnight stays. Parking requirements are 54 spaces based on the square footage of the building. During final site plan review we will submit landscaping, lighting, storm water, utilities. Our plan is to attend the EDC/LDFA meeting on May 15<sup>th</sup> for their comment and review.

Public Comment opened at 5:39 pm

**Michelle Cortright: 1825 Wildwood Harbor Rd.** – How many jobs are proposed?  
**Scott MacKenzie: 847 W. Division St.** – With the amount of traffic anticipated for daily use, 54 parking spaces are indicated, why are there so many?

**Penny Hardy: 437 North St.** – What practice will manage the facility? **Place** – that is not a zoning question for us to answer.

**Adam Graef: 1296 Marshall Rd.** - I also would like to know why so many parking spaces are required for a medical building.

**Monica Ross: 109 E. Michigan Ave.** – Is this an accepted use for the current zoning in the Industrial Park, or do they need a change in zoning to complete their project?

**McPherson** – Back in 2010 the Planning Commission and City Commission recognized that the Industrial Park is not a viable option just for industrial uses any longer, so they moved towards a Business Park with an ordinance amendment done at that time, it brought in a wide variety of uses that are non-industrial to the park.

**Michael Cain: Boyne City City Manager and member of the EDC/LDFA** – These plans will be going before the EDC/LDFA board prior to your next meeting. I have shared the plans with them, and they are very excited about the new business venture. One of the concerns they have is the parking along M75, as it is an entrance into the community, they would like to see the parking moved behind the building with green space along the M75 corridor.

With no further comments, public comment closed at 5:44 pm

#### Board Discussion

**Place** – It is exciting to hear about opportunities coming to Boyne, can you give us an idea on how many people will be involved daily? **Graham** – Really don't have an answer but, believe 10 to 20 people will be staff, nurses and doctors, with patients daily.

**Allen** – Did you look at other locations other than Boyne when you decided to build and did you look at the parking requirements in other cities?

**Graham** – I represent the owner, and did not come on board until after they decided to purchase the property.

**Place** – One of the items brought up was landscaping, screening and the parking. Will landscaping be brought forward in the final plan? **Graham** – Yes

**Ross** – Is there a glazing requirement on the windows? **McPherson** – No specific glazing requirements for the Industrial District

**Ellwanger** – This is a real asset to the community.

**St. Dennis** – I also feel that there is too much asphalt for the estimated daily use

**Place** – We have already talked about reviewing the parking requirements in the ordinance for all districts.

**Biskner** – Can you move the building forward and put parking behind?

**Graham** – It would be difficult, as the corner in the back of the building has a drop off on the site.

With no further comments from the board, the applicants will come back with a final site plan for development plan review, next month

#### Preliminary review 100 N. Lake Street

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Planning Director McPherson reviewed his staff report in the agenda packet, and reiterated that this is a revised site plan for the Lofts of Lake Street, no decisions on this plan can be made tonight, the object is to get input and feedback from the board on these plans, this is a proposed mixed use in the CBD which is a principal permitted use; so is a use by right, you do not have any discretion in regards to the use; specifically issues that deal with that use, such as rents, tax implications, financial viability of plan, whether you think it is a good use or bad use, whether it is an appropriate spot for this use or not. You are charged with looking at the development plan and physical attributes of the site and building and how they relate to the zoning ordinance standards.

**Marilyn Crowley: Michigan Community Capital** - Thank you for giving us an opportunity to return after we gathered feedback, and hope that you see that we really listened and tried to make changes according to your suggestions. We really tried to make this an asset that the community can be proud of. Tonight we are specifically looking for solid feedback to see if this will be a viable project and get an understanding of what the next steps will be.

**Mike Corby: Integrated Architecture** - We have been working with comments received from our last meeting, have met with the Main Street design committee and have taken in all of the comments about the size and mass of the building, lack of parking, façade designs and made changes based on those. We have removed one floor, extended the building to the east, decreased the number of units, and increased the available parking on site, changed up the facades using varying roof lines, proposing change up of various material and window patterns, along with different cornice work. We are exploring on street parking on both Lake and State Streets which will allow us to get an additional 13 spaces that will be public. We have met with the Main Street Design committee and they have given us an ok to pursue the possibility.

Public Comment opened at 5:58 pm

**Skylar MacNaughton: 112 W. Morgan St.** – I do like the new look, but have concerns if this is right thing for Boyne and if the store fronts will be filled. Are there any plans to put in a street light at the intersection?

**Jack Henricks: 2375 Pine Boulevard** – experienced in construction management with Dow Chemical and have done a lot of site work, so giving you my perspective. I have looked at the plans and have concerns with the foot print and building site intensity, site surface is about 95% impervious so will need site surface retention area. The waste handling will be excessive on the current system, there is zero setback on the south side, and there is only one entrance into the parking area, so snow clearing will be tough in the winter time on Lake Street.

**Floyd Wright: Lakeshore Rd.** – I give you compliments on the design, I would be proud to drive by this each day; however, I am against giving any relief for residential parking, snow removal is a huge issue, where will snow piles be stocked, storm water retention is also an issue.

**Ward Collins: 1209 Wildwood Heights Rd.** – You did a great job listening to the community's comments from the last meeting. The greatest advantage is that it will get rid of one of the biggest eye sores in the community and downtown area. I feel that it is a very appropriate use. As far as parking is concerned, our recent parking study that was done last year concludes that we have excess spots in the downtown area.

**Rose Newton: 214 State St.** –The zero setback has not changed, but there is now on street parking, is the city going to put it in as it will be within the right of way? Who will ultimately control those spaces, how do you keep them open for public and not for private residential use? If you continue to give concessions for developers what about the other business owners.

**Barbara Malpass Young: 1003 Hull St.** – 61 parking spots for 42 apartments don't add up, and then with retail or restaurant there is not enough. How is the school bus going to pick up the kids, how are emergency vehicles and trash collectors going to gain access. I also feel that storm water is a concern. We can't fill the stores in town now, how are these going to be filled? I do not want to see empty store fronts and apartment or condos like Charlevoix. I do appreciate that the developer came back after listening to our concerns.

**Steve Roote: 214 State St. & 302 State St.** - with the new development, how many spaces are required now? If they are going to head up State Street, what will stop them from heading into Veterans Park? How many spaces will go up State St?

**Lesley Pritchard: 361 Wildwood Heights Trail Walloon Lake** - I'm glad that the facades have changed. I still have an issue with parking, your numbers don't add up, I feel they are incorrect as 1.5 is a typical allotment for residential units. What about loading and delivery zones, where will they go? Where will the commercial spaces deliver? Emergency services, how are they going to get to the back side of the building? There are supposed to stairwells at the ends of each buildings, I don't see them here, landscaping is not adequate, for setbacks I'm not clear on what is required, are they being met? How can you consider a plan that I feel does not meet your current zoning?

**Penny Hardy: 437 North St.** – I also feel that the parking is inadequate, and am concerned about allowing parking on Lake Street due to snow removal and what will the time limit be to park? Veterans Park across the street draws a lot of people and cars into this area, will there be a stop light at the intersection? Where will the containment be for runoff

**Steve Roote: 214 State St** – What about a cross walk that is marked, or a street light for safety.

**Elizabeth Looze: Snyder Rd.** – I have been involved in Planning for several years and have seen many projects presented and then not come to completion in Petoskey. I fully support this project. The building as presented meets all of the zoning requirements, the developer has been very receptive to your suggestions and comments and made

changes at their expense. This company being a non-profit is willing to take on risk to see this development through.

**Scott MacKenzie: 847 W. Division St.** – Thank you for coming back with modifications to the design with improvements based on community input. One of our city’s goals is to provide all levels of housing, this will not solve our problems, but it is a start. They dropped the design down to 3 floors and changed up the roof line at our suggestions. We have talked about being a walkable community, and this project will do that. It meets a vast majority of the zoning requirements, so ask that you consider this proposal and approve it.

**Tony Cutler – Local Business Manager, and support staff to Senator Schmidt** – We need to make this happen, as far as the statement made that the condos on One Water Street are still vacant, that is not true, they have all been sold. As everyone knows, it is difficult to find available affordable housing, so many of my staff can’t or won’t live here because of the lack of housing or outrageous cost. I believe this is a great project, they have the funding and are willing to take the risk, let them come and build.

**Melissa Casper: 503 Spring St.** – This project could create workforce housing, there are elderly or others who want to live in this type of building which is walkable to everything, have no maintenance upkeep and who do not want to own a vehicle, they would sell their homes which would then be available to purchase, so is a very valuable thing for turnover.

**Pam Macksey: 01110 Jefferson St.** – This developer has met with the Main Street Design Committee a couple of times, and we as a board support this project. There is no affordable housing in this area, and this will provide some of that. It will bring commerce to our area, as it fits zoning, I urge you to fully consider their application.

**Adam Graef: 1296 Marshall Rd.** – They listened to our comments, suggestions and complaints and came back with revised plans, tonight, they just want to know if it is a viable project for them to move forward, as a board you need to look at if it falls within the zoning ordinance, we, as a community need to work with them.

**Bruce Janssen: 111 N. East St.** – We gotta make this work, they listened to our concerns and comments. The Main Street Design Committee has met with the designer and we approved their plans with a couple of suggestions about window designs. This is a win/win for everyone. I am in favor of on street parking on Lake Street and State Street, I believe that parking is not an issue because of all of the surrounding public parking. Figure this out, it’s your job.

**Kelsie King-Duff: Main Street Executive Director** – When developments are within the Main Street district, they are encouraged to meet with the design committee, which is one of the first steps in building a platform between them and our community; we provide recommendations to the Planning Commission with what was discussed and presented to us; they came back with updated designs from all of the suggestions made. The design committee has reviewed the plans and will make a recommendation to approve them with as proposed with a couple minor changes. They incorporated our citizen’s suggestions from before, and the committee has a few to add, change up the designs for the windows, all lighting should be downward, consider landscaping around the entrance, consider burying the overhead power lines, work with the city on the under-utilized alley and come back for final materials and colors.

With no additional comments, Public comment was closed at 6:45 pm with answers to the audience questions.

In the CBD, no residential housing is allowed on the ground floor. Parking within the city right of way, you could control it with signage for 2 hour parking only.

As far as people parking in the private lot, they could have people towed, put up signage, limit the time that deliveries, trash removal and snow removal were done so that areas would be available at those times. As far as the number of required parking spaces for

the development, until final plans are submitted, we do not have an exact required number of spaces. Emergency access and storm water concerns, will be addressed when the department heads review the plans, the drive through is 14' in height. Landscaping plans will also be reviewed when the final plans come before the board. Mike Corby also provided answers to the many questions. By law you must deal with storm water and run off properly. Under the drive through, there is parking that was not indicated on the plans, but the count of 85 spaces is accurate with the additional proposed street parking. We are very close to meeting the parking requirements with the mixed uses of the project. We meet all of the setback requirements, we want the west side (street side) landscaping to be as vibrant as possible, yet still leave the store fronts visible. By code, stairwells have been addressed and designed into the layout.

**Marilyn Crowley** – We have spent thousands of dollars revising the plans to make sure we met the concerns of the community. We have spent additional money as a deposit on the property. If parking is going to be a limiting factor, let us know now before we go to final full site plan review. We appreciate the feedback from the community, but need to know if there is has a possibility of moving forward before we finalize the plans.

#### Board Discussion

Appreciates that the development team listened to the comments and concerns of the citizens and this board; the revision they came back with is leaps and bounds above what was originally presented, they reduced the floor count to 3, they changed up the roof heights, using various materials to mix up the façade'. Without knowing for sure exactly what the commercial spaces will be, it is hard to determine the exact number of parking spaces needed for this mixed use development. The zoning ordinance does give the commissioners the discretion to waive or reduce the number of required spaces based on available public parking, it also allows for up to a 20% reduction for collective uses of the available parking spaces. With the parking study that was recently done, the public parking spaces in the area are only utilized approximately 20 to 25% of the time. It is the developers wish to anchor the retail/commercial space with a restaurant that will help draw to this location, other than that, unknown exactly what else will go in that space.

The board was satisfied with the changes that were presented, they meet the zoning ordinance in setback, height and landscape. It will bring vibrancy to the downtown area, will create jobs, addresses the lack of housing and beautify long stagnant properties. The perceived issue of lack of parking on site, can be handled with the available public parking in the area. The board felt that the on street parking was a good idea and believe that it would help the retail and commercial businesses.

**Marilyn Crowley** – We like the on street parking, we will fund it; it will be owned by the city to maintain and manage however the city wants to. It will help us with our project and further address parking.

The board indicated they would like to see the project move forward as proposed with the design and parking, and feel that no problem is insurmountable to overcome. The development team will be back next month with complete site plans.

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Staff has reached out to Annika for a consultation proposal for an overview of the parking study, how to apply it within various projects. As an RRC Community the hopes is that it will be fully funded, staff has not heard anything back from our request as of yet. May need to schedule a special meeting to have this session, poll from the board is that a Tuesday night would work the best.

#### Staff Report

## Good of the Order

- With more projects that have been before this board, and working as a team, it puts stress on applicants, commissioners, staff and everyone if there is a lack of attendance is for several meetings in a row, and meetings have to be cancelled due to a lack of a quorum. The board understands life happens and understand occasional absences, but is there a way to work this out?
- Is the city considering an ordinance regulating VRBO units? One of the City Commission goals was to identify impacts of these units, and if ordinances should be developed. Staff is in the process of collecting data on these and in the future may have something for the board to review, consider and possibly recommend to the City Commission.
- What will happen with the log cabin chamber building? At this point, nothing is in the works for it.
- Storm water structures slated on the new Cedar St and Terrace St. project, how will they work? They will help reduce the amount of pollutants that are flushed downstream by reducing the amounts of liquid materials that are put into the watershed by french drain flushing and allowing water to seep into the ground first with a catch basin to collect materials.

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The next regular meeting of the Boyne City Planning Commission is scheduled for Monday, May 20, 2019 at 5:00 p.m.

**2019-4-15-10**

**St. Dennis moved, Biskner seconded, PASSED UNANIMOUSLY** a motion to adjourn the April 15, 2019 meeting at 7:46 pm

## Adjournment

**\*\*Motion**

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Chair Aaron Place

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Recording Secretary Pat Haver

# CITY OF BOYNE CITY

**To:** Chair Aaron Place and fellow Planning Commissioners

**From:** Scott McPherson, Planning Director

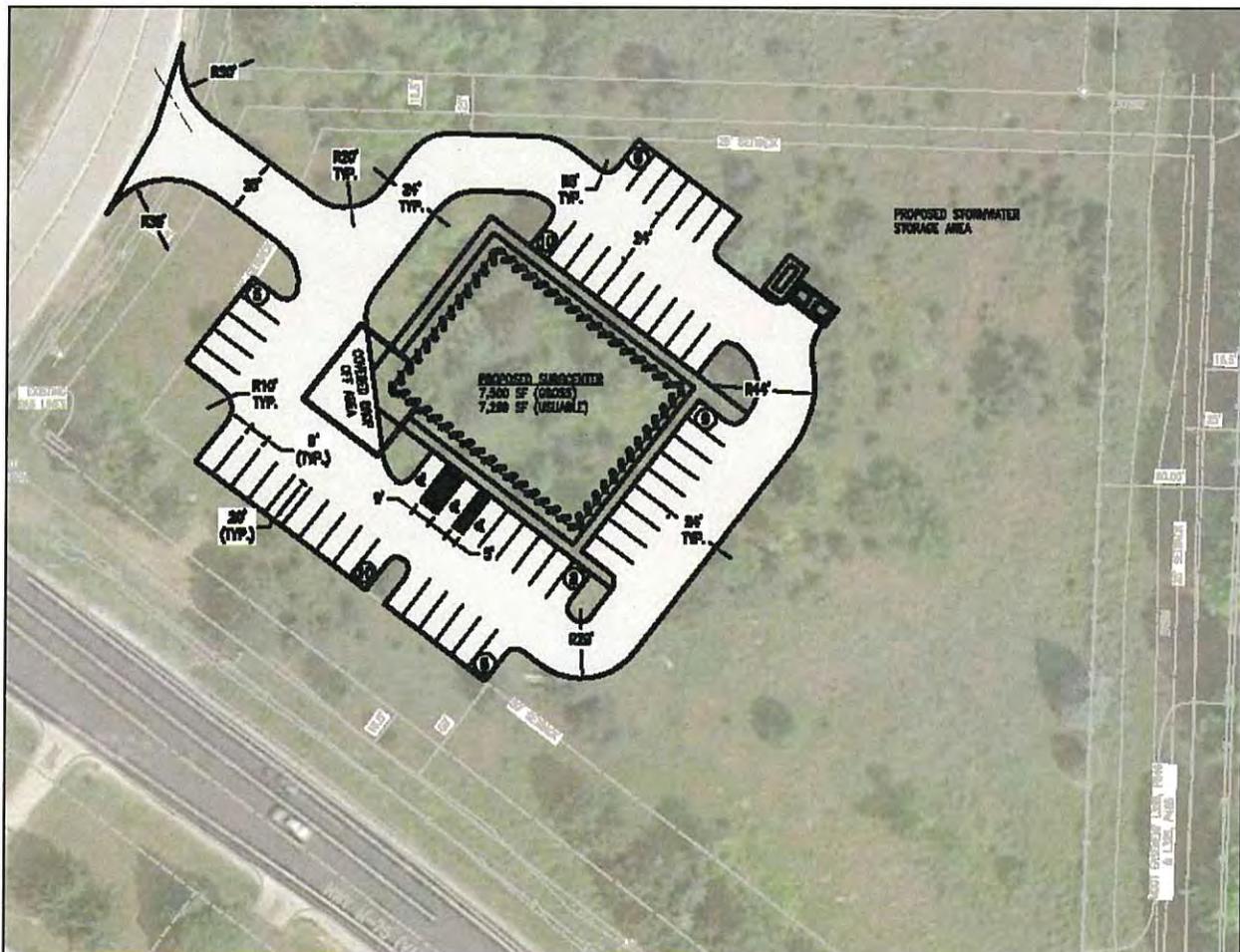
**Date:** May 20, 2019

**Subject:** SurgCenter Development Plan Review



## BACKGROUND

An application for development plan review for Boyne City SurgCenter located at 825 Moll Drive on lot 16 in Air Industrial Park Phase II. The parcel is approximately 4.16 acres and is located on the corner of M-75 and Moll Drive. The property is located in the Planned Industrial District (PID) and the application is to develop a 7,500 sqft surgical clinic facility. The proposed use is principal permitted use in the PID district as provided under sections 14.20(J) of the Boyne City Zoning Ordinance (BCZO).



## **DISCUSSION**

The proposed use is a principal allowed use in the PID district and the proposed site plan is in compliance with all ordinance requirements of BCZO section 20.20 in regards to building placement, size, height and lot coverage. Section 14.40 and 22.30 of the BCZO establishes the design standards for buildings located in the PID district and the proposed complies with the applicable design criteria and the building is similar in character to existing buildings in the industrial park. The design, layout and location of the proposed driveway meets ordinance criteria. A landscaping buffer is located around the perimeter of the site and interior landscaping islands in accordance with ordinance criteria have been provided. Site lighting plan and fixture specifications have been provided and are in accordance with ordinance standards. The required parking spaces for the proposed use is 53 spaces (7 spaces per 1000 gfa =  $7500/1000 \times 7 = 52.5$ ) and 54 spaces have been provided. The site will have one access point on Moll Drive. At the preliminary review the on April 15<sup>th</sup> there were comments in regards to the amount of required parking and that it seemed excessive. This was also the opinion of EDC/LDFA and it was their recommendation to the Planning Commission to consider a reduction in the amount of developed parking. Section 24.10(B)(3) of the BCZO does provide discretion to the Planning Commission to approve the development of less parking spaces, which is as follows:

*Variance and Deferment. The Planning Commission may, without proof of unnecessary hardship, waive the requirements of this Article if the Planning Commission finds from the evidence presented that the intended use of a proposed building does not require parking or loading facilities to the degree specified herein. However, the Planning Commission shall require that adequate open areas be retained around such a building to permit development of the required parking or loading areas should the use of the building change at a later date. The site plan shall note the area where parking is being deferred, including dimensions and a dotted parking lot layout.*

In this case the property does have areas the can be retained around the building which would permit the development of additional parking if determined it is needed.

## **PROCESS**

The application requires development plan review as per article 19 Development Plan Requirements. As the project is in the industrial park a review of the project by the EDC/LDFA is required and was completed on May 13<sup>th</sup> see attached minutes. Plans were also distributed to all department heads and the city manager for review and no issues or concerns were noted.

## **RECOMMENDATION**

The Planning Commission should then review the development plan requirements. On each item of the findings the Commission needs to make a determination based on the relevant facts if the standard is met, not met or met with conditions. The Planning Commission can approve the application, approve the application with conditions or deny the application. If approved with conditions the conditions must be listed, if denied the reasons for denial must be stated.

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
BOYNE CITY SURGCENTER**

**Section 19.40 Development Plan Approval Criteria.**  
In order that buildings, open space and landscaping will be in harmony with other structures and improvements in the area, and to ensure that no undesirable health, safety, noise and traffic conditions will result from the development, the Planning Commission shall determine whether or not the development plan meets the following criteria, unless the Planning Commission determines that one or more of such criteria are inapplicable:

<b>ORDINANCE REQUIREMENT</b>	<b>FACTS</b>	<b>FINDINGS</b>
<p><u>A. General.</u> All elements of the development plan shall be designed to take into account the site's topography, the size and type of plot, the character of adjoining property, and the traffic operations of adjacent streets. The site shall be developed so as not to impede the normal and orderly development or improvement of surrounding property for uses permitted in this Ordinance. The development plan shall conform with all requirements of this Ordinance, including those of the applicable zoning district(s).</p>	<p>Lot 16 of Air Industrial Park Phase II Lot is 4.16 acres with 661'.of frontage on M-75 and 100' frontage on Moll Dr. Lot is mostly flat with scrub vegetation, some trees on south lot line could be preserved for parking lot screening. East 60' of parcel encumbered with MDOT drainage easement. Proposed 7,500 sqft single story building located approximately in middle of lot, meets all setback, building, and lot coverage requirements per BCZO section 20.02. Surgical clinic is a principle permitted use as per BCZO 14.20(J). Site</p>	
<p><u>B. Building Design.</u> The building design shall relate to the surrounding environment in regard to texture, scale, mass, proportion, and color. High standards of construction and quality materials will be incorporated into the new development. In addition to following design guidelines adopted in specific district or sub-area plans, the building design shall meet the architectural and building material requirements of this Ordinance.</p>	<p>Building is approximately 80 x 90 single story structure with hip roof. Building will have parking and drive lane around perimeter of building with main entry and drop off protected by a canopy on the south west corner of the building. Structure will have stone accent wall and siding and is typical in look and character of existing buildings in the industrial park.</p>	
<p><u>C. Preservation of Significant Natural Features.</u> Judicious effort shall be used to preserve the integrity of the land, existing topography, and natural, historical, and architectural features as defined in this Ordinance, in particular wetlands designated /regulated by the Michigan Department of Environmental Quality, and, to a lesser extent, wetlands which are not regulated by the Department.</p>	<p>Site has no significant natural features.</p>	
<p><u>D. Streets.</u> All streets shall be developed in accordance with the City of Boyne City Subdivision Control Ordinance and City Municipal Standards, unless developed as a private road in accordance with the requirements of the City.</p>	<p>Not Applicable</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
BOYNE CITY SURGCENTER**

<p><u>E. Access, Driveways and Circulation.</u> Safe, convenient, uncongested, and well defined vehicular and pedestrian circulation within and to the site shall be provided. Drives, streets, parking and other elements shall be designed to discourage through traffic, while promoting safe and efficient traffic operations within the site and at its access points. All driveways shall meet the design and construction standards of the City. Access to the site shall be designed to minimize conflicts with traffic on adjacent streets, particularly left turns into and from the site. For uses having frontage and/or access on a major traffic route, as defined in the City of Boyne City Comprehensive Plan, the number, design, and location of access driveways and other provisions for vehicular circulation shall comply with the access management provisions of this Ordinance.</p>	<p>Parcel accessed from Moll drive by a single 25' access drive. Access from M-75 is prohibited. Plan meets access management standards.</p>	
<p><u>F. Emergency Vehicle Access.</u> All buildings or groups of buildings shall be arranged so as to permit necessary emergency vehicle access as required by the Fire Department, Ambulance Department and Police Department.</p>	<p>Site accessibility has been reviewed by BCPD, Fire Dept and Ambulance Dept, necessary access for emergency vehicles is provided.</p>	
<p><u>G. Sidewalks, Pedestrian and Bicycle Circulation.</u> The arrangement of public or common ways for vehicular and pedestrian circulation shall be connected to existing or planned streets and sidewalks/pedestrian or bicycle pathways in the area. There shall be provided a pedestrian circulation system which is separated from the vehicular circulation system. In order to ensure public safety, special pedestrian measures, such as crosswalks, crossing signals and other such facilities may be required in the vicinity of primary and secondary schools, playgrounds, local shopping areas, fast food/service restaurants and other uses which generate a considerable amount of pedestrian or bicycle traffic.</p>	<p>Not Applicable</p>	
<p><u>H. Barrier-Free Access.</u> The site has been designed to provide barrier-free parking and pedestrian circulation.</p>	<p>Site has barrier free access.</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
BOYNE CITY SURGCENTER**

<p><u>I. Parking.</u> The number and dimensions of off-street parking [spaces] shall be sufficient to meet the minimum required by this Ordinance. However, where warranted by overlapping or shared parking arrangements, the Planning Commission may reduce the required number of parking spaces, as provided in this Ordinance.</p>	<p>Site meets ordinance requirements for parking number and layout. If the planning commission finds evidence the amount of parking is unnecessary, the planning commission may waive the requirements of Article 24.30 if areas on the site can be retained for future parking development if needed.</p>	
<p><u>J. Loading.</u> All loading and unloading areas and outside storage areas, including refuse storage stations, shall be screened in accordance with this Ordinance.</p>	<p>Loading and outside refuse storage located on north side of building. Screening details provided</p>	
<p><u>K. Landscaping, Screening, and Open Space.</u> The landscape shall be preserved in its natural state, insofar as practical, by removing only those areas of vegetation or making those alterations to the topography which are reasonably necessary to develop the site in accordance with the requirements of this Ordinance. Landscaping shall be preserved and/or provided to ensure that proposed uses will be adequately buffered from one another and from surrounding public and private property. Landscaping, landscape buffers, greenbelts, fencing, walls and other protective barriers shall be provided and designed in accordance with the landscaping provisions of this Ordinance. Recreation and open space areas shall be provided in all multiple-family residential and educational developments.</p>	<p>Landscape plan provided.</p>	
<p><u>L. Soil Erosion Control.</u> The site shall have adequate lateral support so as to ensure that there will be no erosion of soil or other material. The final determination as to adequacy of, or need for, lateral support shall be made by the Planning Director or City Engineer, and have a valid Charlevoix County Soil Erosion permit.</p>	<p>Soil erosion permit required.</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
BOYNE CITY SURGCENTER**

<p><u>M. Stormwater Management.</u> Appropriate measures shall be taken to ensure that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Provisions shall be made to accommodate stormwater which complements the natural drainage patterns and wetlands, prevent erosion and the formation of dust. Sharing of stormwater facilities with adjacent properties shall be encouraged. The use of detention/retention ponds may be required. Surface water on all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic or create standing water. All such measures shall comply with the Charlevoix County Stormwater Ordinance.</p>	<p>Site will have on site storm water detention with overflow connection to existing city stormwater system.</p>	
<p><u>O. Lighting.</u> Exterior lighting shall be arranged so that it is directed preferably downward onto the subject site and deflected away from adjacent properties. Lighting shall not impede the vision of traffic along adjacent streets. Flashing or intermittent lights shall not be permitted.</p>	<p>Site lighting plan provided.</p>	
<p><u>P. Noise.</u> The site has been designed, buildings so arranged, and activities/equipment programmed to minimize the emission of noise, particularly for sites adjacent to residential districts.</p>	<p>Proposed use is not anticipated to create any noise exceeding what can be expected in the district.</p>	
<p><u>Q. Mechanical Equipment.</u> Mechanical equipment, both roof and ground mounted, shall be screened in accordance with the requirements of this Ordinance.</p>	<p>Mechanical equipment located on north side of building.</p>	
<p><u>R. Signs.</u> The standards of the City of Boyne City's Sign Ordinance are met.</p>	<p>Sign permit required.</p>	
<p><u>S Hazardous Materials or Waste.</u> For businesses utilizing, storing or handling hazardous material such as automobile service and automobile repair stations, dry cleaning plants, metal plating industries, and other industrial uses, documentation of compliance with state and federal requirements shall be provided.</p>	<p>Medical facility waste.</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
BOYNE CITY SURGCENTER**

<p><u>T. Other Agency Reviews.</u> The applicant has provided documentation of compliance with other appropriate agency review standards, including, but not limited to, the Michigan Department of Natural Resources, Michigan Department of Environmental Quality, Michigan Department of Transportation, Charlevoix County Drain Commissioner, Northwest Michigan Community Health Agency, Charlevoix County Building Department, and other federal and state agencies, as applicable.</p>	<p>Development plan reviewed by EDC/LDFA on May 13, 2019. All other required permits must be obtained.</p>	
<p><u>U. Approval Process.</u> The development plan shall be reviewed by the Planning Commission. If disapproval is recommended, the Planning Commission shall cite reasons for such disapproval. If the Planning Commission finds a development plan not in conformity with this section, it may, at its discretion, return the development plan to the applicant with a written statement of the modifications necessary to obtain approval. Upon resubmission of the modified development plan, the Planning Commission shall review the plan. The Commission may approve, disapprove or approve subject to compliance with such modifications and conditions as may be deemed necessary to carry out the purpose of this Ordinance and other ordinances and resolutions of the City. If disapproved, the Planning Commission shall cite reasons for such disapproval.</p>		



Approved \_\_\_\_\_

**MEETING OF  
May 13, 2019**

**MINUTES OF THE BOYNE CITY ECONOMIC DEVELOPMENT CORPORATION  
MEETING DULY CALLED AND HELD ON MONDAY, May 13, 2019 at Precision  
Edge ~ 1448 Lexamar Drive, Boyne City**

**CALL TO ORDER**

Chair Gillett called the meeting to order at 12:00 p.m.

**ROLL CALL**

Present: Kelly Bellant, Michael Cain, Pete Friedrich, Ralph Gillett, Josette Lory  
Absent: Pat Anzell, Michelle Cortright, Drew May

**EXCUSED ABSENCES  
MOTION**

**Cain moved, Friedrich seconded, PASSED UNANIMOUSLY** to excuse Pat Anzell, Michelle Cortright and Drew May.

**MEETING ATTENDANCE**

Staff: Recording Secretary Jane Halstead  
Public: Two

**MINUTE APPROVAL  
MOTION**

**Cain moved, Bellant seconded, PASSED UNANIMOUSLY** to approve the minutes of January 28, 2019.

**HEARING CITIZENS  
PRESENT**

None.

**UNFINISHED BUSINESS  
SurgCenter Site Plan Review**

Jason Marnin of SurgCenter Development and Darren Graham of Gosling Czubak were on hand for a preliminary review of the site plan for the proposed SurgCenter to be built in the Business Park. The proposed surgery center will be a 7,500 square foot, out-patient, orthopedic surgery office for elective surgeries. They anticipate employing 12 to 14 staff members in addition to 9 to 12 rotating physicians and will operate during normal business hours.

The preliminary site plan includes 53 parking spaces. In order to eliminate the number of cars visible from M-75 and reduce the water runoff, the Board would like to see the 16 parking spaces to the South indicated in the plan held in reserve for future needs and the area landscaped instead. The Board would also like exterior lighting to be reduced during non-operational hours.

The developer would like to break ground this summer and open in March of 2021. The SurgCenter project will be presented to the Boyne City Planning Commission on May 20<sup>th</sup> at 5:00 p.m.

**Cain moved, Friedrich seconded, PASSED UNANIMOUSLY** to recommend the Boyne City Planning Commission approve the preliminary SurgCenter site plan as presented with the amendment to not build, and instead landscape and hold in reserve for future needs, the South 16 parking spaces shown in the plans with credit given for the new landscape proposed and, additionally, provide for exterior building light reduction during non-business hours.



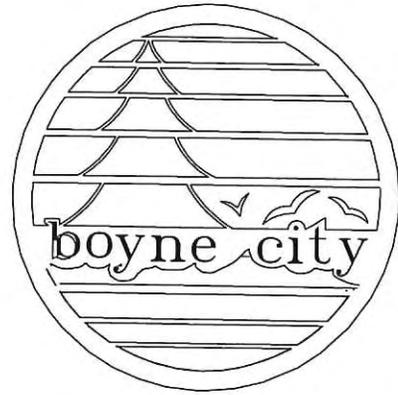
# CITY OF BOYNE CITY

**To:** Chair Aaron Place, and fellow Planning Commissioners

**From:** Scott McPherson, Planning Director

**Date:** May 20, 2019

**Subject:** Lofts on Lake Street Development Plan Review



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## Background Information

The properties located at 100 and 120 N Lake Ave and 114 State Street are owned by BC3 Ventures LLC and Still Point Trust and are under contract for purchase by Michigan Community Capital. The three parcels, PID's 15-051-240-028-50, 15-051-240-030-10 and 15-051-240-054-00 have a total land area of approximately 40,400 square feet with 227 feet of frontage on Lake Street and 198 feet of frontage on State Street. The property is zoned Central Business District (CBD). A 27,075 sqft 3 story mixed used building is being proposed which will consist of a 9,025 sqft commercial space on the first floor and total 42 apartments on the 2<sup>nd</sup> and 3<sup>rd</sup> floors. The apartments will consist of 6 studio, 28 single bedroom, and 8 two bedroom units. Currently tenants for the commercial space have not been identified. The commercial space can accommodate a wide variety potential uses and it is anticipated that a mix of retail, office and restaurant uses will occupy the space.



## Discussion

In the CBD a mixed use development is principle permitted use as per section 10.20(M) of the Boyne City Zoning Ordinance (BCZO). While the proposed use is a use by right and the Planning Commission has no discretion in terms of the proposed use, it may be beneficial for the Planning Commission to review the intent and purpose of the CBD and the applicable City planning documents that include or reference the project site.

As per section 10.10 A-L, the CBD is designed and intended for the following:

- A. *Encourage innovative, traditional and neo-traditional commercial and mixed use developments.*
- B. *Encourage a lively social environment and economically viable downtown with a wide variety of uses in a pedestrian-oriented setting, with shared parking.*
- C. *Extend greater opportunities for traditional community living, working, housing and recreation to all citizens, residents, and visitors of the city.*
- D. *Encourage a more efficient use of land and public services, and to reflect changes in technology of land development by directing new development in a traditional, compact, and consolidated pattern of mixed use and varied commercial styles.*
- E. *Reduce the excessive sprawl of development and the segregation of land uses that cause unnecessary traffic congestion.*
- F. *Prohibit the development of drive-in and drive-through facilities, which contributes to traffic congestion and poses a threat to the pedestrian environment.*
- G. *Discourage the development of separate off-street parking facilities for each individual use, and to encourage the development of off-street parking facilities designed to accommodate the needs of several individual uses.*
- H. *Prohibit uses that do not deal directly with consumers and are disruptive to pedestrian activities.*
- I. *Promote the creation of urban places which are oriented to the pedestrian thereby promoting citizen security and social interaction.*
- J. *Promote developments where the physical, visual and spatial characteristics are established and reinforced through the consistent use of compatible urban design and architectural design elements which improves the visual character of the downtown. Such elements shall relate to the design characteristics of an individual structure or development, to other existing and planned structures or developments in a harmonious manner, resulting in coherent overall design and development patterns for the downtown.*
- K. *Prohibit commercial and business uses that create objectionable noise, glare, odors, or other nuisances.*
- L. *Encourage development of an urban Main Street with mixed land uses, shared parking, and continuous frontage which not only serves the needs of the immediate neighborhood, but also the City and surrounding areas as a whole.*

As per the Future Land Use map in the 2015 Boyne City Master Plan the subject parcels are located within the Downtown Core. The intent and purpose for properties located in the Downtown Core is described in the plan as follows:

***“Downtown Core—**The downtown and historic core is the focal point of Boyne City providing a mix of retail, office, residential, and public uses, supported by a transportation system that creates a pedestrian friendly atmosphere. This area provides easy access to local businesses with an enhanced streetscape environment. This plan promotes continued mixed-use development in the Downtown Core to reinforce the unique identity and attractive pedestrian environment. This land use category is intended to encourage commercial uses, small-scale retail shopping, entertainment uses, convenience stores, office, and personal and business service uses. Residential uses are encouraged on upper floors of commercial buildings. Building heights should generally not exceed three stories, except where it can be demonstrated that additional height will not alter the historic character of the downtown. Brick, stone and masonry will be the primary building materials in this area to give a sense of permanence.”*

The 100 N Lake Street parcel is specifically identified in the 2015 Boyne City Master Plan as a priority redevelopment site. The Boyne City Master Plan describes Priority Redevelopment Sites as follows:

*“Priority Redevelopment Sites - As part of the RRC (Redevelopment Ready Community) certification process the City was required to assemble and prioritize potential redevelopment sites. To accomplish this task the City completed an inventory and review of all the potential redevelopment sites in the City. The criteria used for inclusion into the priority list was the properties needed to be located in or close to the downtown, needed to be currently vacant or underutilized land and/or buildings, and the properties had attributes that set the site apart such*

as historic designation, blight, location, architecture or other unique characteristics that would make it an important and significant property.”

As a RRC designated priority site the 100 N Lake Street property has been advertised and promoted for redevelopment by the City and by the State through the MEDC Redevelopment Ready Communities “Opportunity Michigan high priority redevelopment sites across the state.”



**redevelopment ready communities** **OPPORTUNITY MICHIGAN**  
*high priority redevelopment sites across the state*

## REGION 2

Northwest region



### Lally Building

100 North Lake Street, Boyne City, MI 49712



This currently vacant building is located close to downtown with views of Lake Charlevoix and offers a commercial redevelopment opportunity. Property has 111 feet of frontage on Lake Street with ingress and egress from Lake and Park streets. Veterans Park is directly across the street.

**Owner:** Patrick and Carolyn Lally

**Contact:** Pat O'Brien Realty  
231.582.1700, pat@patobrien.com

**City contact:** Michael Cain  
231.582.0377, mcain@boynecity.com

**Zoning:** Downtown mixed-use

**Lot size:** 0.35 acres

**Building size:** 5,000 square feet

**State equalized value:** \$76,700

**Utilities:** Water, sewer, electricity, natural gas, fiber, cable

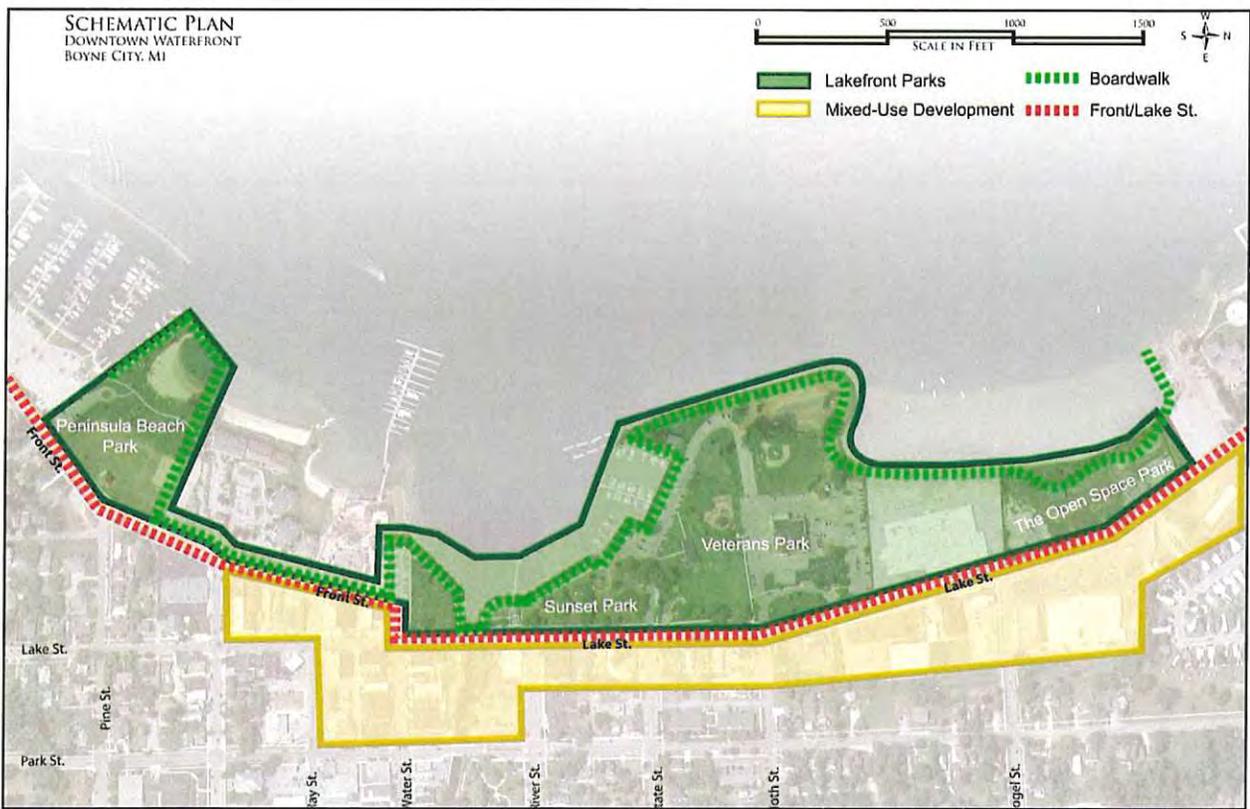
**Certification:** Main Street: Master



The project area is included in the 2006 Waterfront Master Plan and the plan states the following for this area: *“Lake Street from the River to Vogel Street - This area is highly suitable for redevelopment that introduces more residential units into the downtown core and retail and other nonresidential as market may bear. The area from the river to State Street is suggested for mixed use.”* The plan also included a conceptual design for this area and it is shown below:



The project area is also included in the 2017 Boyme on the Water Plan which designates the east side of Lake Street for mixed use.



The project site was also specifically included in the 2018 Boyne City Parking Study as a potential future development site in forecasting the 5 year scenario. The scope of the forecasted development for the project site is as follows: *“Site 1 - Potential development (2-3 sty mixed use) on a privately owned site on block 4. 3 floors = 69,960sf (23,320sf per floor) of mixed use and 22 parking spaces.”* The proposed use is 3 stories, 27,075 square feet of mixed use with a total of 82 new parking spaces (69 private 13 public).

The Boyne on the Water Plan also addresses the issue of parking and states the following: *“By eliminating parking minimums, adopting shared parking agreements, exploring pricing on-street and off-street parking in a variable way, and reducing requirements for parking for individual developments, Boyne City can create a more vibrant, walkable, and dense core that will compliment waterfront development and allow more space along the waterfront to be devoted to human-scale use rather than asphalt and parking spaces.”*

As mentioned previously, the proposed mixed use is a principle permitted use in the CBD and as such the Planning Commission does not have discretion regarding the use. The proposed building placement, height, and lot coverage meet all the applicable requirements of BCZO Article XX schedule of regulations. In addition to these requirements the project is subject to the CBD design requirements of BCZO section 10.50 and the development plan requirements of BCZO Article XIX. The Planning Commission’s authority and discretion is limited to the application of the specific standards listed in these referenced sections.

One area the Planning Commission does have discretion is provided in BCZO section 19.40(T) in determining the amount of parking the development should provide. The proposed parking for the proposed use is less than the ordinance standards of BCZO Article 24 and the applicants have requested the Planning Commission approve the reduction and/or waiver of spaces as provided by BCZO sections 24.20(F) and 10.50(M).

At the preliminary review on April 15, 2019, the Planning Commission reviewed the proposed plan and was asked specifically by the development team if the proposed parking was sufficient for the proposed uses. It was the consensus of the Commission that the proposed parking plan would be sufficient and indicated that the 20% reduction as per BCZO section 24.20 (F) was applicable and that there was rationale for reducing the parking requirement as per BCZO section 10.50(M). The specific ordinance sections are as follows:

*Section 24.20(F) In the instance of dual function of off-street parking spaces where operating hours of buildings do not overlap and there is an opportunity for a patron to visit more than one use, the Planning Commission may grant a reduction to the required number of spaces of up to twenty percent (20%) provided a signed agreement is provided by the property owners.*

*Section 10.50(M) The Planning Commission shall determine if the number of off-street parking and loading/unloading spaces required per Article XXIV of this Ordinance shall be met, or if a lesser number spaces or no spaces are required due to the following: the availability of on-street parking spaces, off-site parking lots, or municipal parking lots; a finding that patrons will either walk to the site from nearby neighborhoods, or will park at other sites and visit several uses at one time; or the placement and configuration of existing buildings.*

While the Planning Commission did indicate at the preliminary review the parking as proposed was sufficient, a formal finding is still required for plan approval. In this case the exact parking space requirements of the proposed use cannot be determined as the end user of the commercial space is unknown. Given this unknown, assumptions were necessary to calculate parking space counts. As the proposed commercial space is expected to have a mix of commercial uses that are anticipated to include retail, office and restaurant uses, the parking requirements were calculated by dividing the commercial space into 3 equal parts and calculating individual parking counts for each of the anticipated uses. The calculations are as follows:

Retail	1 space for every 250 sqft gfa x 3,008 sqft	12 spaces
Office	1 space for every 350 sqft gfa x 3,008 sqft	9 spaces
Restaurant	1 space for every 100 sqft gfa x 3,008 sqft	30 spaces
2 Bedroom Apartment	2 spaces per unit x 8 units	16 spaces
Studio and 1 Bedroom	1 ½ spaces per unit x 34 units	<u>51 spaces</u>
Total		118 spaces

The application of the 20% reduction as per 24.20(F) would reduce the total of the calculated parking to 94 spaces. The proposed project will include the development of 69 off street private parking spaces and 13 on street public parking for a total of 82 new parking spaces. While the on street spaces will remain public, the addition of the spaces to the overall parking supply can be one of the factors the Planning Commission considers in making a finding that a lesser number of parking spaces are needed as per 10.50(M). Taking into account the 20% reduction as per 24.20(F) and the 82 provided parking spaces, the proposed development would require a waiver of 12 spaces.

The Planning Commission can also refer to the 2018 Boyne City Parking Study in making a finding. The study included assessments of the overall study area and the core downtown area. The property is located outside the core area and is specifically shown as Development Site 1 on map 6 page 33 of the Parking Study. The Study says the following about parking turnover and occupancy and parking demand.

*TURNOVER AND OCCUPANCY SUMMARY- The number of spaces occupied at peak time in downtown Boyne City are relatively low. The peak overall occupancy was 39% with 647 of the 1,622 spaces occupied. When we analyzed the results for the core area we see that the peak overall occupancy increases to 52%, with 408 of the 782 spaces occupied. This tells us that there is still sufficient parking in the downtown area available during peak hours, though all parking may not be available for all users. Additionally, the public parking may not be located as the most convenient spaces for all destinations.*

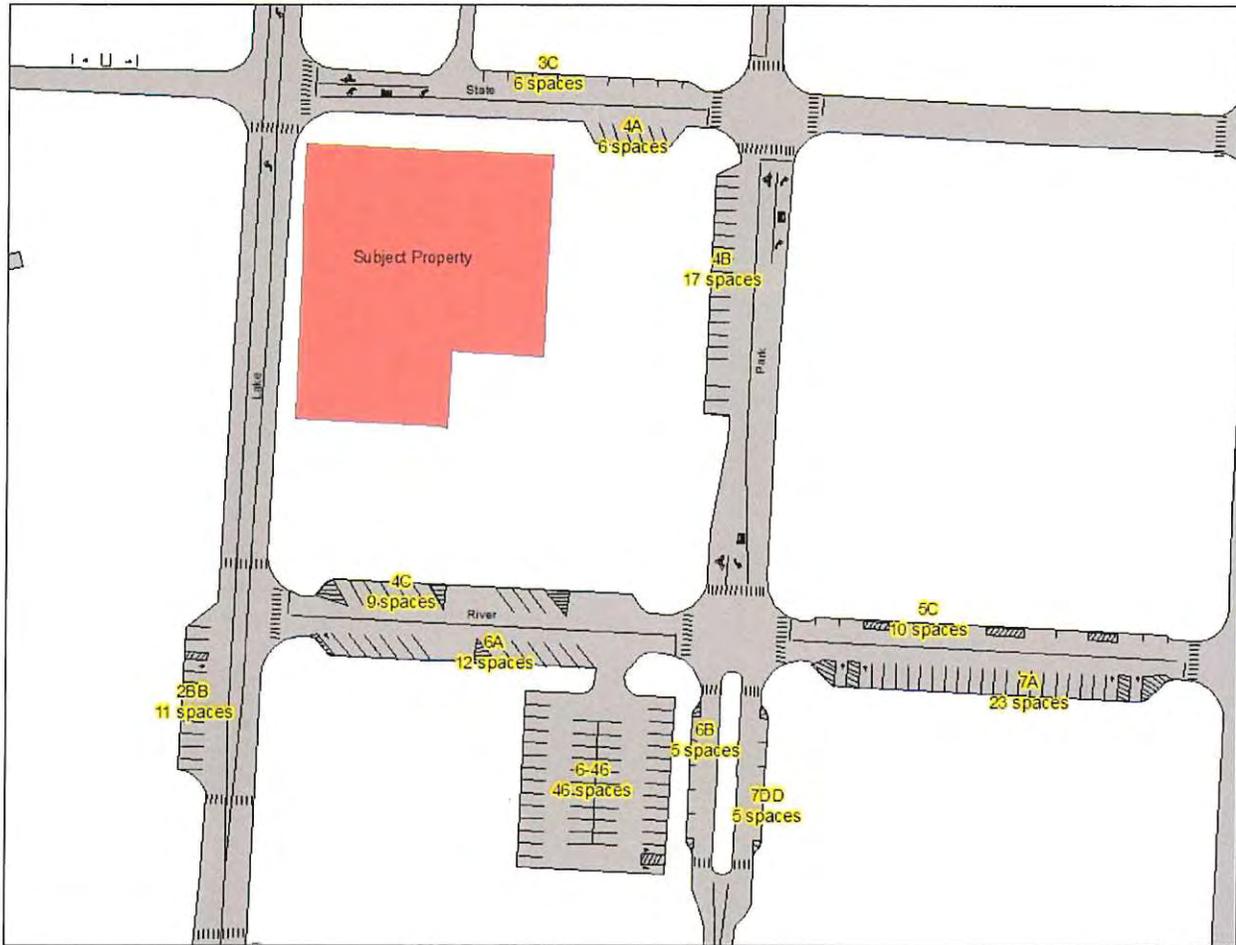
*PARKING DEMAND - The current daytime parking situation in the entire study area as calculated showed an overall surplus of 765 spaces. When looking at the core area this surplus is only 302 spaces. Currently there is parking located within a couple of blocks of all areas to handle shortages. As development continues and additional businesses come to downtown Boyne City, there is the potential for an increase in the intensity (number of people visiting each land use) and of overall land use. Therefore, it is important to constantly monitor the parking system and update the demand model with any changes to the parking supply or land use. The updated model should then be compared to occupancy counts from the parking system.*

As suggested by the parking demand summary, the parking demand model was updated to include the impacts of the proposed development. For this block the existing parking demand was calculated at 37 spaces and the existing parking supply for the block was calculated at 72 spaces for a net existing surplus of 35 spaces. (Please note these values are slightly different than shown in table H pg 26 of the parking study. The existing demand was adjusted to include the 5,000 sqft warehouse space for the 100 N Lake St building and existing parking supply increased from 70 to 72 as there are 17 on street spaces on Park Street not 15). Inputting the proposed uses into the model would increase demand for the block to 94 spaces and increase the supply to 137 spaces for a net surplus of 43 spaces. The proposed development has significantly less floor area (27,075 vs 69,960) and significantly larger number of parking spaces (82 vs 22) than was modeled for this site in the 5 year forecast. Due to these differences the proposed development has a net surplus of 43 spaces as opposed to the forecasted deficit of 103 spaces that was shown in the 5 year scenario (map 5.1 pg 30).

In addition to the parking demand model the Planning Commission can consider the availability of public parking areas in the vicinity of the project. An inventory of all public parking spaces that are available to the general public within a two minute walk of the parcel was completed. The inventory did not include spaces that were private, handicap or reserved. Within a one minute walk (350') of the site there are 108 public parking spaces, extending out to a two minute walk (700') there are over 300 public spaces available. To gauge the current use of spaces in close proximity to the project, counts were obtained for several of the public parking areas around the subject parcel. Counts were obtained by referring to aerial



A map of the counted parking areas and table of showing the average number and percentage of spaces used and available are shown below:



Lot Name	Total Spaces	Average Spaces Used	Average Percent Used	Average Spaces Available	Average Percent Available
3C	6	3.26	54.3%	2.74	45.74%
4A	6	1.34	22.3%	4.66	77.66%
4B	17	2.60	15.3%	14.40	84.73%
6A	12	0.30	2.5%	11.70	97.52%
6-46	46	18.15	39.5%	27.85	60.55%
6B	5	1.28	25.5%	3.72	74.47%
2BB	11	2.00	18.2%	9.00	81.82%
7A	23	4.09	17.8%	18.91	82.24%
7DD	5	0.32	6.4%	4.68	93.62%
4C	9	1.17	13.0%	7.83	87.00%
5C	10	0.87	8.7%	9.13	91.28%
All	150	35.36	23.6%	114.64	76.43%

**PROCESS**

The application requires development plan review by the Planning Commission as per article 19 Development Plan Requirements. A preliminary review of the proposed plan was completed by the Planning Commission on April 15<sup>th</sup>. As the project is in the Mainstreet District a review of the project by the Main Street Design Committee is required and was completed on April 2<sup>nd</sup>.

**RECOMMENDATION**

The Planning Commission should review the applicable standards of BCZO Article 19 Development Plan Requirements and Article 10 Central Business District Design Requirements and make a determination based on the relevant facts if the standard is met, not met or met with conditions. If approved with conditions the conditions must be listed, if denied the reasons for denial must be stated.

## Existing Conditions

Column1	Column2	Column3	Column5	Column6	Column7	Column8	Column82	Column9	Column10	Column103	Column104	Column11	Column102	Column12	Column13	Column19	Column20
Daytime Parking Demand Matrix																	
A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R
TOTALS	OFFICE	MEDICAL OFFICE	RETAIL	SERVICE	MIXED USE	RESTAURANT/ BAR	HOTEL	RESIDENTIAL	LIBRARY	PARK	MARINA	WAREHOUSE	CHURCH	VACANT	DEMAND	PARKING	SURPLUS/
							(PER ROOM)	(PER UNIT)		(PER ACRE)	(PER BERTH)				CURRENT	SUPPLY	DEFICIT
PARKING GENERATION RATIOS	1.85	2.15	1.65	1.75	2.00	4.00	1.15	1.00	1.50	1.50	0.59	0.75	0.35	2.25			CURRENT
1	71,400	-	-	-	-	-	-	-	-	-	-	-	-	-	132	176	44
2	-	-	-	-	-	-	-	-	-	7	42	-	-	-	35	136	101
3	-	14,503	-	-	-	2,160	-	-	-	-	-	-	-	6,232	40	122	82
4	11,882	-	2,604	-	-	-	-	3	-	-	-	10,000	-	1,102	37	72	35
5	-	1,500	-	-	-	-	-	-	-	-	-	3,700	-	-	6	28	22
6	2,000	-	20,472	4,952	-	6,060	-	2	-	-	-	-	6,000	-	74	122	48
7	5,510	-	3,609	-	-	6,371	-	1	-	-	-	-	-	-	43	65	22
8	-	-	-	-	-	15,000	-	10	-	-	-	-	-	-	70	77	7
9	1,355	1,116	3,176	4,192	-	-	-	1	-	-	-	-	-	5,064	18	37	19
10	1,139	-	21,711	-	6,708	5,200	-	2	-	-	-	-	-	5,250	74	110	36
11	-	-	3,000	1,200	-	-	-	-	-	-	-	12,502	-	4,297	16	90	74
12	-	-	-	-	-	-	33	-	-	-	-	-	-	-	38	51	13
13*	-	-	-	-	-	-	-	-	-	6	-	-	-	-	9	69	60
14	-	-	-	7,000	-	20,540	-	16	-	-	-	-	-	3,751	110	77	-33
15	5,948	-	2,424	956	-	-	-	5	-	-	-	-	-	-	22	37	15
16	-	-	-	-	-	-	-	-	10,000	-	-	2,804	4,000	-	19	73	54
17	-	-	1,000	-	-	-	-	-	-	-	-	-	-	2	29	27	
18	-	-	1,452	-	14,000	-	-	3	-	-	-	-	4,000	-	35	80	45
19	1,024	3,546	1,056	-	-	-	-	9	-	-	-	750	10,000	5,124	24	65	41
20	-	-	-	-	-	-	-	-	-	-	-	-	45,000	-	16	65	49
<b>TOTALS</b>	<b>100,258</b>	<b>20,665</b>	<b>60,504</b>	<b>18,300</b>	<b>20,708</b>	<b>55,331</b>	<b>33</b>	<b>52</b>	<b>10,000</b>	<b>13</b>	<b>42</b>	<b>29,756</b>	<b>69,000</b>	<b>30,820</b>	<b>820</b>	<b>1,581</b>	<b>761</b>
															(STALLS)	(STALLS)	(STALLS)

\*Block 13 Residential units and associated parking have been removed from the parking demand.

\*Block 21 is not included in the parking demand

Existing demand for Block 4 updated to increase warehouse to 10,000 sqft to include 5,000 sqft for 100 N Lake bldg. Existing parking supply increased from 70 to 72, as there are 17 on street spaces on Park Street, not 15.

Proposed

Column1	Column2	Column3	Column5	Column6	Column7	Column8	Column82	Column9	Column10	Column103	Column104	Column11	Column102	Column12	Column13	Column19	Column20
Daytime Parking Demand Matrix																	
A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R
TOTALS	OFFICE	MEDICAL OFFICE	RETAIL	SERVICE	MIXED USE	RESTAURANT/ BAR	HOTEL	RESIDENTIAL	LIBRARY	PARK	MARINA	WAREHOUSE	CHURCH	VACANT	DEMAND	PARKING	SURPLUS/
PARKING GENERATION RATIOS	1.85	2.15	1.65	1.75	2.00	4.00	(PER ROOM)	(PER UNIT)		(PER ACRE)	(PER BERTH)				CURRENT	SUPPLY	DEFICIT
																	CURRENT
1	71,400	-	-	-	-	-	-	-	-	-	-	-	-	-	132	176	44
2	-	-	-	-	-	-	-	-	-	7	42	-	-	-	35	136	101
3	-	14,503	-	-	-	2,160	-	-	-	-	-	-	-	6,232	40	122	82
4	13,136	-	5,612	-	-	3,008	-	45	-	-	-	5,000	-	-	94	137	43
5	-	1,500	-	-	-	-	-	-	-	-	-	3,700	-	-	6	28	22
6	2,000	-	20,472	4,952	-	6,060	-	2	-	-	-	-	-	6,000	74	122	48
7	5,510	-	3,609	-	-	6,371	-	1	-	-	-	-	-	-	43	65	22
8	-	-	-	-	-	15,000	-	10	-	-	-	-	-	-	70	77	7
9	1,355	1,116	3,176	4,192	-	-	-	1	-	-	-	-	-	5,064	18	37	19
10	1,139	-	21,711	-	6,708	5,200	-	2	-	-	-	-	-	5,250	74	110	36
11	-	-	3,000	1,200	-	-	-	-	-	-	-	12,502	-	4,297	16	90	74
12	-	-	-	-	-	-	33	-	-	-	-	-	-	-	38	51	13
13*	-	-	-	-	-	-	-	-	-	6	-	-	-	-	9	69	60
14	-	-	-	7,000	-	20,540	-	16	-	-	-	-	-	3,751	110	77	-33
15	5,948	-	2,424	956	-	-	-	5	-	-	-	-	-	-	22	37	15
16	-	-	-	-	-	-	-	-	10,000	-	-	2,804	4,000	-	19	73	54
17	-	-	1,000	-	-	-	-	-	-	-	-	-	-	-	2	29	27
18	-	-	1,452	-	14,000	-	-	3	-	-	-	-	4,000	-	35	80	45
19	1,024	3,546	1,056	-	-	-	-	9	-	-	-	750	10,000	5,124	24	65	41
20	-	-	-	-	-	-	-	-	-	-	-	-	45,000	-	16	65	49
TOTALS	101,512	20,665	63,512	18,300	20,708	58,339	33	94	10,000	13	42	24,756	69,000	29,718	878	1,646	768
															(STALLS)	(STALLS)	(STALLS)

\*Block 13 Residential units and associated parking have been removed from the parking demand.

\*Block 21 is not included in the parking demand

Increased office by 1,254sqft to 13,136. 3,008 added - 1,754 removed (bank).  
 Increased retail to 5,612 sqft. 3,008 added to 2,604 existing.  
 Increased restaurant to 3,008 sqft. 3,008 added to 0 existing  
 Decreased vacant to 0. 1,102 sqft removed (Bikefix).  
 Warehouse decreased to 5,000 sqft. 5,000 sqft removed (Lally).  
 Residential units increased to 45. 42 new units added to 3 existing.  
 Total parking supply increased to 137. (92 private + 45 on street public)  
 Private parking supply increased to 92. (34 existing + 69 new -11 removed)  
 Public parking supply increase to 45. (32 existing + 13 new)

May 14, 2019

Michael Cain  
City Manager  
319 N. Lake Street  
Boyer City, MI 49712

RE: Review of the Lake Street Development Plan, Parking Review

Dear Mr. Cain,

Cambourne Consulting was hired by the Michigan Municipal League in behalf of Boyne City, to review the parking needs for the Lake Street Development Plan. We were tasked to review the parking requirements for Boyne City and then compare the findings to the Rich & Associates parking study to determine if the project is providing enough parking for the development.

The report from the Boyne City Planning Director to the Planning Commission is detailed, easy to understand and thorough. The report provides information regarding the turn over and occupancy study conducted by Rich & Associates along with a separate occupancy study conducted by staff. A brief recap of the information is provided within this letter.

The Lake Street Development Plan Calls for 36 residential units and 9,025 sqft of commercial space. The project plans for 69 private parking spaces in the lot behind the building and 13 public spaces will be developed on street, totaling 82 spaces. The current plan does not detail how much of the 9,025 sqft of commercial space will be used for office, restaurant and retail. At this point we will assume the developer will use 3,008 sf (the total equally divided by 3) for each land use.

City Calculations

Retail	1 space for every 250 sqft gfa x 3,008 sqft	12 spaces
Office	1 space for every 350 sqft gfa x 3,008 sqft	9 spaces
Restaurant	1 space for every 100 sqft gfa x 3,008 sqft	30 spaces
2 Bedroom Apartment	2 spaces per unit x 8 units	16 spaces
<u>Studio and 1 Bedroom</u>	<u>1.5 spaces per unit x 34 units</u>	<u>51 spaces</u>
Total		118 spaces

The total parking need for this development (with assumptions) is 118 spaces. Zoning (24.20 (F)) allows for a reduction of 20% reducing the total calculated parking need to 94 spaces. With the 13 new on-street spaces this only leaves the development 12 spaces short. Because this development is located in the downtown area where public parking is provided, there are 331 public parking spaces located within a reasonable walking distance (350 ft = one-minute walk and 700 ft = two-minute walk). Shared use reductions are a common zoning practice in pedestrian friendly and walkable downtown settings to help regulate an over development of parking.

Calculations based on Rich & Associates Parking Study

Retail	3,008 sqft/1,000 x 1.65	5 spaces
Office	3,008 sqft/1,000 x 1.85	6 spaces
Restaurant	3,008 sqft/1,000 x 4.00	13 spaces
<u>Residential</u>	<u>1 space per unit x 42 units</u>	<u>42 spaces</u>
Total		66 spaces

The parking demand model for Rich & Associates was developed as a tool to determine the affects of potential changes or development on the downtown area. It was not recommended that Boyne City adopt this model, only use it as a tool to plan for the future. This model accounts for shared use parking, thus reductions should not be made.

Many downtowns do not require parking within a downtown setting, instead the municipality will take on the responsibility of providing the parking. This parking demand model is set up for a community that does not have parking requirements for the downtown and one that has prioritized parking management to make the parking more efficient.

The overall conclusion is that this development is 12 parking spaces short of the zoning requirements. Though, when examining the turnover and occupancy numbers it is clear that there is enough parking within a reasonable walking distance. The Planning Commission can determine the amount of parking that is needed for a development (BCZO section 10.50 (M)). In this case, the site has more than adequate available public parking within a one-minute walk to fill the demand for the additional 12 spaces that the development needs.

As always, it is a pleasure working with the Boyne City staff. Please contact me with any questions.

Sincerely,

Annaka Norris  
Cambourne Consulting  
Cambourneconsulting@gmail.com



**RESOLUTION**

**In support the Lofts on Lake Street Development Project**

**WHEREAS**, the non-profit, Michigan Community Capital (MCC), has submitted plans for the redevelopment of three parcels at the intersection of North Lake and State Streets, calling their project Lofts on Lake Street, and

**WHEREAS**, MCC has presented their plans to the Main Street Design Committee for their review and recommendation, and

**WHEREAS**, MCC has incorporated the input the Main Street Design Committee into their most recent plans earning an unanimous recommendation for approval by it, and

**WHEREAS**, the proposed project will redevelop three currently underutilized parcels, and

**WHEREAS**, the mixed used proposal is consistent with the visions of the community driven Boyne City Master Plan, the Waterfront Master Plan, the Boyne on the Water Plan and Boyne City Zoning Ordinance, and

**WHEREAS**, in addition to being consistent with the Boyne City Master Plan this site has been specifically called out in it for redevelopment statewide as part of Michigan's Redevelopment Ready Communities Program, and

**WHEREAS**, the 2018 Boyne City Parking Study found there to be more than sufficient existing public parking to support a much larger project on this specific site providing much less parking than currently proposed, and

**WHEREAS**, the Lofts on Lake Street development will help create a continuation of downtown Boyne City on North Lake Street, and

**WHEREAS**, the development of 42 units of residential space and 9,000 square foot of retail space will have a positive impact on the Boyne City Main Street district and the entire Boyne City community on a number of levels including helping to meet the City's 2018-2020 specific

goals of Keeping Boyne City's Economy Strong and Resilient and Increasing Housing availability, and

**WHEREAS**, the housing units included in this project meet the goals of Boyne Housing Solutions, a grassroots group of community members working towards more attainable housing for the residents of Boyne City, and

**WHEREAS**, the income-based, long term rentals at the Lofts on Lake Street will create attainable housing for the local workforce, and

**WHEREAS**, by locating the project downtown it builds on Boyne City's historic downtown's mixed uses, provides for a more walkable community, provides housing that will be available to support both downtown employees and the business they will visit, uses already existing infrastructure, does not encourage sprawl, and

**WHEREAS**, thoughtful economic development that preserves the character of our community, as well as helps foster a sense of pride, develops a diverse, stable business environment, and involves collaboration of community groups and organizations together meet the mission of Boyne City Main Street,

**NOW, THEREFORE, BE IT RESOLVED** by the Boyne City Main Street board that Boyne City Main Street endorses and supports the efforts of Michigan Community Capital to redevelop the property at 100 and 120 North Lake Street for the Lofts on Lake Street project, as proposed in the plans dated April 15, 2019, and encourages others to review all the relevant materials and do the same.



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Chairman, Boyne City Main Street



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Executive Director, Boyne City Main Street

May 2, 2019



**To:** Scott McPherson, Planning Director  
Boyne City Planning Commission

**From:** Kelsie King-Duff, Executive Director

**Date:** April 3, 2019

**Subject:** Design Committee Recommendation for Lake Street Development

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**OVERVIEW:**

When a new development is proposed within the Boyne City Main Street District, developers are encouraged to meet with the Boyne City Main Street Design Committee prior to seeking approvals from the Planning Commission. These meetings focus on the overall look and aesthetic of the building and the committee works with the developer to encourage a design that fits within the Boyne City community. The developers of the Lake Street Development (the former First Merit site and adjacent properties) have met with the Design Committee on two occasions. On April 1<sup>st</sup> an updated design was brought to the committee for review after updating the project with suggestions from the first meeting. The committee met again on April 2<sup>nd</sup> and made the recommendation outlined below. The plans have not been reviewed by the Boyne City Main Street board at this time.

**RECOMMENDATION:**

Bruce Janssen, seconded by Pam Macksey, made a motion that the planning commission approve the plan as proposed, with consideration given to the comments below. All voted in favor.

- A. The developers did a lot to incorporate the suggestions from previous meetings, and from public feedback. Overall, the committee really liked the complete redesign.
- B. The building provides a nice transition in the building materials used in the core business district and those used in the rest of the Main Street district along Lake Street.
- C. The developer is encouraged to give consideration to the design of the windows, especially on the south half of the building and explore something more symmetrical.

- D. The lighting on the building should be down-lit, so the sidewalks are lit and not the side of the building.
- E. No high landscaping should be placed near the entry/exit for vehicles.
- F. Consideration should be given to burying all overhead utility lines on the site and along the roads.
- G. The developers are encouraged to work with the City on a plan for the currently underutilized alley that is adjacent to the property.
- H. The developers are encouraged to come back to the committee once the rear and side of the building design is completed, as well as when color choices are made for brick, Hardie siding, and other façade materials.

Adam Graef, seconded by Becky Harris, also made a motion to recommend that for the benefit of the project and the community, the development create parallel parking, without bump outs, behind the existing curb line on the East side of North Lake Street. All voted in favor. Having no bump outs or midblock sidewalk connection to Lake Street will allow for easier winter maintenance and help to safely control pedestrian traffic to proper crosswalks.

It should be noted that upon further investigation since this motion was made there may not be enough road right-of-way to put the preferred on-street parking behind the curb line. A survey may be needed to clarify.

The Design Committee looks forward to working further with the developer and Planning Commission.



May 14, 2019

City of Boyne City  
Attn: Aaron Place, Planning Commission Chair  
319 N Lake St  
Boyne City MI 49712

Re: Site Plan Review Application  
Lofts on Lake Street  
100 & 120 Lake St N

Dear Mr. Place,

We recently received feedback from Planner, Scott McPherson, upon his review of our plan. We would like to offer clarification to his questions:

**1. The off-street parking was reduced from 72 to 69, what was the reason for the reduction?**

The rearrangement of the parking lot to accommodate emergency vehicle access from the alley and the addition of greenspace for a landscape buffer reduced the onsite parking to 69 spaces. There are also 13 street parking spaces that will be available for the development. The total parking provided is 82 spaces. This number satisfies the parking requirements for the residential and commercial demands with the 20% mixed-used parking reduction (a breakdown of the parking provided is below).

**Residential**

- (34) 1br & studio @ 1 space/unit = 34 spaces
- (8) 2br @ 2 space/unit = 16 spaces

**Commercial**

- (3,008 sf) Restaurant @ 1 space/100 sf = 30 spaces
- (3,008 sf) Retail @ 1 space/250 sf = 12 spaces
- (3,008 sf) Office @ 1 space/350 sf = 9 spaces

Total Parking required = 101 spaces

20% Mixed-Use Parking reduction = 81 spaces

Parking Provided = 82 spaces

**2. Will there be an interior storm water drainage system for the building?**

Yes

Integrated Architecture

4090 Lake Drive Grand Rapids Michigan 49546 tel 616.574.0220 fax 616.574.0953 www.intarch.com

**3. Mechanical equipment is not shown, where will it be located?**

Mechanical equipment is located on each residential floor as well as the east side of the roof near the elevator overrun.

**4. The 3d concepts show planters along Lake street but none are shown on the landscape plan. Will planters be provided? If so, please provide details.**

There are curbed planter beds along Lake St and landscaped areas around the common building entrance and the corner of Lake and State. We will provide more information on the movable planter boxes that are planned for the north half of the building along Lake St.

**5. It may be helpful to bring a sample board of façade building materials to the Planning Commission meeting.**

We have samples of brick and the fiber cement board that can be presented.

**6. In the ROW along Lake and State street lighting matching the existing street lights used in the City will be required. Looks like the best placement would be to have one on the corner and two spaced at the middle and end of the property down each street for a total of 5 fixtures.**

This can be accommodated.

**7. A loading zone is not shown, will there be a dedicated loading area in the parking lot?**

There is not one currently on the plan. This can be addressed through a dedicated restricted parking space along the street with hours of loading availability indicated by signage.

**8. It is noted that the dumpster will be screened with vegetation, the planning commission may prefer a wall.**

A masonry dumpster enclosure (of similar appearance to building masonry) with a gate will be provided.

**9. As part of the review, the Planning Commission will be reviewing the ordinance standards of 10.5 and 19.4. It may be beneficial to provide a narrative to the PC addressing these standards.**

The design has been updated and revised to meet the ordinance standards through revisions to scale, materials, proportion and other archetype features/detail that have been incorporated into the design.

**10. Access to the building (clarification to response to EMS Director)**

All first-floor commercial users will have direct access from Lake, State and parking lot sides.

Thank you very much for your time.

Sincerely,



Michael Corby, FAIA, LEED AP  
Executive Vice-President

**Scott McPherson**

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**From:** Mark Fowler  
**Sent:** Monday, May 13, 2019 10:32 AM  
**To:** Scott McPherson  
**Subject:** RE: Lofts on Lake St Plan

Scott

I have no issues. There is sufficient capacity. There is an 8 inch Sewer on Lake St and Sate St. There is a 6 inch water main on Lake St and an 8 inch water main on State St.

Mark

**From:** Scott McPherson  
**Sent:** Wednesday, May 08, 2019 7:36 AM  
**To:** Dept. Heads; Michael Cain  
**Subject:** Lofts on Lake St Plan

Attached are the Lofts on Lake Street Plans submitted for development plan review by the Planning Commission on May 20<sup>th</sup>. Please provide me any comments on the plans by May 15<sup>th</sup>.

Scott McPherson  
Planning Director  
City of Boyne City  
231.582.0343

## Memo

**TO:** McPherson, Scott

**FROM:** John M Lamont, EMS Director

**Date:** May 8, 2019

**RE:** Loft on Lake Street Plan

Thank-you for providing me the opportunity to review the building plan layout.

- Height of the canopy entrance; I understand that it will be tall enough for the fire equipment to enter.
- Access to the building; will there be a common hallway, or will the mixed-use have separate entries, off of N. Lake Street, with access also off of the parking lot?
- Service elevator?



May 9, 2019

City of Boyne City  
Attn: Aaron Place, Planning Commission Chair  
319 N Lake St  
Boyne City MI 49712

Re: Site Plan Review Application  
Lofts on Lake Street  
100 & 120 Lake St N

Dear Mr. Place,

We recently received feedback from EMS Director, John Lamont, upon his review of our plan. We would like to offer clarification to his three questions:

1. ***Height of the canopy entrance; I understand that it will be tall enough for the fire equipment to enter?***

The canopy clearance to the parking lot from State Street is 14'-0". There is additional site access with no overhead clearance issues from the alley on the southeast corner of the lot.

2. ***Access to the building; will there be a common hallway or will the mixed-use have separate entries off of N Lake Street with access also off the parking lot?***

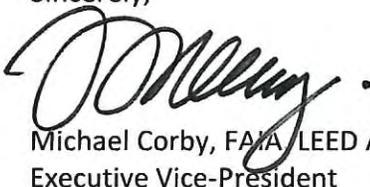
There is a common lobby that is accessed from both the N Lake Street side as well as the parking lot. This lobby has access to the elevator. The stair towers both egress directly to the outside.

3. ***Service elevator?***

There is not a separate service elevator; however, the residential elevator is sized to accommodate a stretcher.

Thank you very much for your time.

Sincerely,



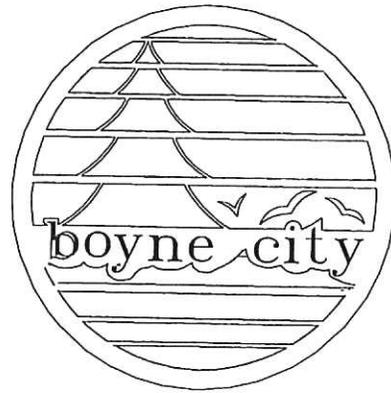
Michael Corby, FAIA, LEED AP  
Executive Vice-President

Integrated Architecture

4090 Lake Drive Grand Rapids Michigan 49546 tel 616.574.0220 fax 616.574.0953 www.intarch.com

## CITY OF BOYNE CITY

To: City Manager and Department Heads  
From: Scott McPherson Planning Director  
Date: May 7, 2019  
Subject: Dept Head Review Lofts on Lake Street



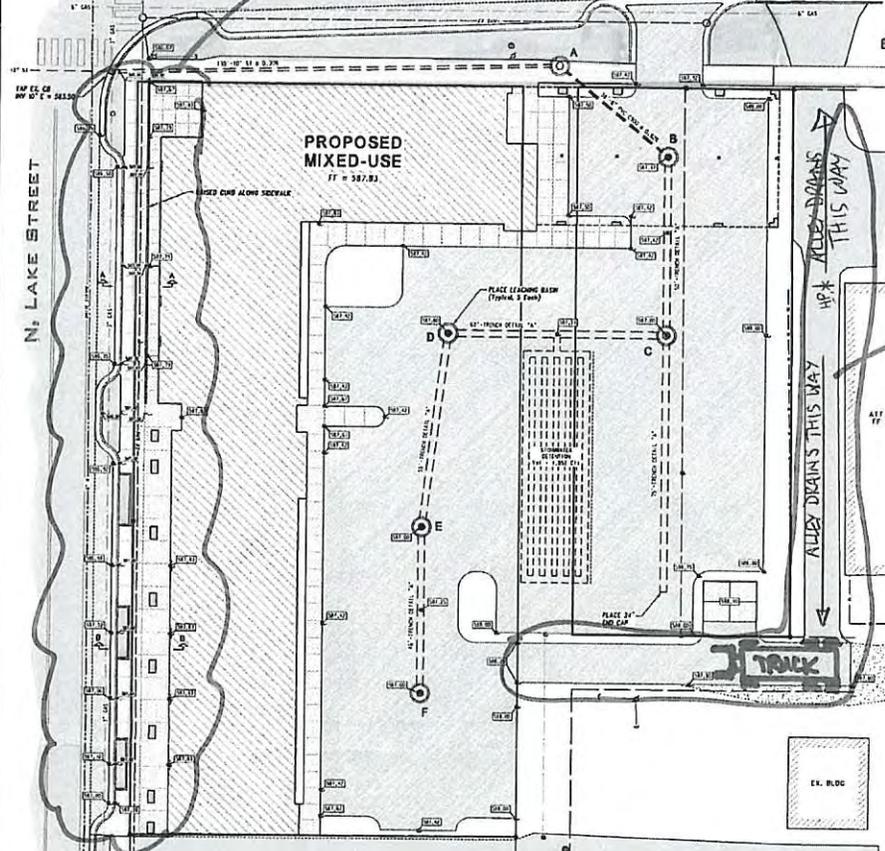
I have attached a copy of the proposed development plan for Lofts on Lake Street. Please review the prints and provide me any comments you may have. The planning commission will be review the plan at their May 20<sup>th</sup> meeting, please have any comments to me no later than close of business May 14<sup>th</sup>. Let me know if you have any questions.

- ① Drainage of the existing + proposed alley?
- ② Sidewalk along N. Lake Street to be maintained by the owner including any portions in public ROW due to street scape impediments. Our machine would be difficult to navigate through there and we have no place to pile snow.
- ③ Dumpster enclosure location does not easily facilitate Front-End Loader collection requiring hard 90° turn into enclosure + no room to back-up. Suggest 45° angle or relocate elsewhere on-site.

*No Place to blow snow into windows with trackless*



**PROPOSED GRADING & DRAINAGE PLAN**  
 BC3 Ventures, LLC Property  
 Boyne City Site Plan Submittal 06 May 2019  
 Part of SECTION 35, T33N, R6W  
 BOYNE CITY, CHARLEVOIX COUNTY, MI



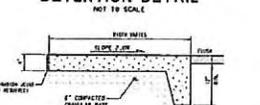
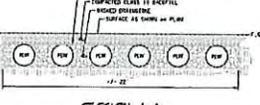
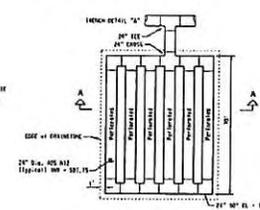
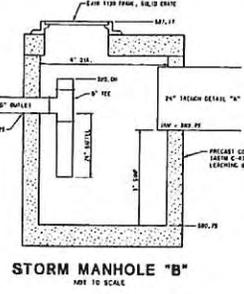
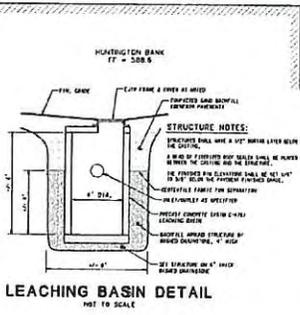
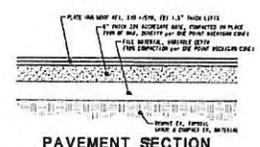
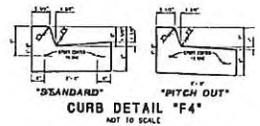
**STORM WATER CALCULATIONS**

STORM	AREA (SQ FT)	COEFFICIENT	RAINFALL (IN)	RETENTION VOLUME (CU FT)
STORM "A"	10,000	0.5	0.5	2,500
STORM "B"	15,000	0.5	0.5	3,750
STORM "C"	20,000	0.5	0.5	5,000
STORM "D"	25,000	0.5	0.5	6,250
STORM "E"	30,000	0.5	0.5	7,500
STORM "F"	35,000	0.5	0.5	8,750
<b>TOTAL</b>	<b>135,000</b>	<b>0.5</b>	<b>0.5</b>	<b>34,250</b>

*how is this area done?*

**RETENTION VOLUME PROVIDED:**

STRUCTURE	VOLUME (CU FT)
LEACHING BASIN A	2,500
LEACHING BASIN B	3,750
LEACHING BASIN C	5,000
LEACHING BASIN D	6,250
LEACHING BASIN E	7,500
LEACHING BASIN F	8,750
<b>TOTAL</b>	<b>34,250</b>



- LEGEND:**
- SIGN
  - UTILITY POLE w/ CUT WIRE
  - TELEPHONE PEDESTAL
  - CURB/STEP
  - POLE
  - FIRE HYDRANT
  - STORM SEWER CATCH BASIN
  - MANHOLE COVER
  - MONITOR WELL
  - PROPERTY LINE
  - FENCE
  - APPROXIMATE WATERMAIN
  - APPROXIMATE SANITARY SEWER
  - APPROXIMATE STORM SEWER
  - FOUND SURVEY POINT AS DESCRIBED
  - FOUND SURVEY REINTERSECTION AS DESCRIBED
  - EX. SPOT ELEVATION
  - EX. CONCRETE SURFACE
  - EX. ASPHALT SURFACE
  - EX. GRAVEL SURFACE
  - PROPOSED ASPHALT SURFACE
  - PROPOSED CONCRETE SURFACE
  - PROPOSED GRADE

**NOTES:**  
 UTILITIES LINES SHOWN SHALL BE CONSIDERED APPROXIMATE LOCATION  
 WATER & POWER UTILITY LINES SHOWN WERE TAKEN FROM DATA PROVIDED  
 BY THE CITY OFP.  
 CALL MISS DIG TREE (3) FULL WORK DAYS PRIOR TO EXCAVATION.

**BENCHMARK ENGINEERING**  
 3000 BAYVIEW BLVD  
 BOYNE CITY, MI 49711  
 (517) 832-1171  
 benchmark@benchmarkeng.com

**PROPOSED GRADING & DRAINAGE PLAN**  
 BC3 Ventures, LLC Property  
 Boyne City Site Plan Submittal



May 16, 2019

City of Boyne City  
Attn: Aaron Place, Planning Commission Chair  
319 N Lake St  
Boyne City MI 49712

Re: Site Plan Review Application  
Lofts on Lake Street  
100 & 120 Lake St N

Dear Mr. Place,

We recently received feedback from the DPW. We would like to offer clarification to these questions:

**1. Drainage of the existing & proposed alley?**

The existing north-south alley will remain undisturbed and will function as it does now. Stormwater will not be discharged onto the existing alley. The east/west alley will be partially paved with drainage to the east as existing. It is not anticipated that the small amount of new paved area will generate additional stormwater to require a new catch basin.

**2. Sidewalk along N Lake St to be maintained by the owner including any portions in public ROW due to streetscape impediments. Our machine would be difficult to navigate through there and we have no place to pile snow.**

It is understood that the sidewalk will be maintained by property maintenance.

**3. Dumpster enclosure location does not easily facilitate front-end loader collection requiring hard 90° turn into enclosure and no room to back up. Suggest 45° angle or relocate elsewhere on site.**

The dumpster can be turned 45°

Thank you very much for your time.

Sincerely,

Michael Corby, F.A.A., LEED AP  
Executive Vice-President

Integrated Architecture

4090 Lake Drive Grand Rapids Michigan 49546 tel 616.574.0220 fax 616.574.0953 www.intarch.com



**Boyne City Fire  
Department**

[www.boynecity.com](http://www.boynecity.com)

(231) 582-6597

319 North Lake Street

Boyne City, MI 49712

Fax: (231) 582-5705

Voicemail: (231) 582-3642

[boynefire@boynecity.com](mailto:boynefire@boynecity.com)

---

From: Dennis Amesbury/ Fire Chief  
To: Scott McPherson  
Subjet: Fire access for the Loft.  
Date: 5/01/2019

Access to building for fire meets our requirements.

Thanks

Dennis Amesbury/ Fire Chief

## Scott McPherson

---

**From:** Mike Long <partpac@gmail.com>  
**Sent:** Wednesday, April 17, 2019 3:54 PM  
**To:** Scott McPherson; Michael Cain  
**Subject:** Parking issue for Lk. St. development

Dear Scott, Michael , and all commissioners,

I am Michael Long, and I own Par T Pac on State and Park St. corner. I also own the 2 properties south of the corner, that abut the alleyway which cuts through the Lalley building from Lk.st. through to Park St. My property essentially mirrors that of the development proposed on Lk. st., with the ATT building separating our respective properties. I only bring this up to let you know, that the parking requirements could be met with some , (or all) of my CBD property . I don't see why a parking issue should arise if the developers could meet all requirements by purchasing adjoining property on the alley.

So,....before allowing the developer to proceed w/out a proper number of parking spaces, I believe you folks should take into consideration that I have the property and space that would fix their parking problem, and they therefore should not be allowed to skirt our parking requirements. I have had many negative issues with the city on being NOT informed, and do not wish to create a stir, but it seems the parking issue could be easily avoided. In fact, the developers could even be presented with an opportunity to increase the size of their retail and residential development.

Please consider my thoughts before eliminating the parking requirements.

Thank you, Michael Long

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
LOFTS ON LAKE STREET**

**Section 19.40 Development Plan Approval Criteria.**  
In order that buildings, open space and landscaping will be in harmony with other structures and improvements in the area, and to ensure that no undesirable health, safety, noise and traffic conditions will result from the development, the Planning Commission shall determine whether or not the development plan meets the following criteria, unless the Planning Commission determines that one or more of such criteria are inapplicable:

<b>ORDINANCE REQUIREMENT</b>	<b>FACTS</b>	<b>FINDINGS</b>
<p><u>A. General.</u> All elements of the development plan shall be designed to take into account the site's topography, the size and type of plot, the character of adjoining property, and the traffic operations of adjacent streets. The site shall be developed so as not to impede the normal and orderly development or improvement of surrounding property for uses permitted in this Ordinance. The development plan shall conform with all requirements of this Ordinance, including those of the applicable zoning district(s).</p>	<p>The subject parcels are located at 100 N Lake. 120 N Lake and 114 State St. The parcel is zoned CBD and is approximately 40,400 sqft. Each parcel has been previously developed and all existing structures and site elements will be removed. The proposed project is a 3 story mixed use building comprised of a 9,025 sqft of commercial space on the first floor, the 2<sup>nd</sup> and 3<sup>rd</sup> stories have a total of 42 residential units comprised of 6 studio units, 28 one bdrm, and 8 two bdrm. Adjacent properties are AT&amp;T switching station and Huntington Bank office. Primary vehicle access to the site will be from State street. 2 existing curb cuts from Lake St and 1 from State St will be eliminated. Emergency access will be available from the alley connecting to Park St.</p>	
<p><u>B. Building Design.</u> The building design shall relate to the surrounding environment in regard to texture, scale, mass, proportion, and color. High standards of construction and quality materials will be incorporated into the new development. In addition to following design guidelines adopted in specific district or sub-area plans, the building design shall meet the architectural and building material requirements of this Ordinance.</p>	<p>Building is a 3 story with a flat roof. Conceptual elevations show mixture of materials to be used for façade. Design guidelines for CBD apply. Plan reviewed and recommended for approval by Main Street Design Committee.</p>	
<p><u>C. Preservation of Significant Natural Features.</u> Judicious effort shall be used to preserve the integrity of the land, existing topography, and natural, historical, and architectural features as defined in this Ordinance, in particular wetlands designated /regulated by the Michigan Department of Environmental Quality, and, to a lesser extent, wetlands which are not regulated by the Department.</p>	<p>The site does not have any significant natural features</p>	
<p><u>D. Streets.</u> All streets shall be developed in accordance with the City of Boyne City Subdivision Control Ordinance and City Municipal Standards, unless developed as a private road in accordance with the requirements of the City.</p>	<p>Not Applicable, no new streets proposed</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
LOFTS ON LAKE STREET**

<p><u>E. Access, Driveways and Circulation.</u> Safe, convenient, uncongested, and well defined vehicular and pedestrian circulation within and to the site shall be provided. Drives, streets, parking and other elements shall be designed to discourage through traffic, while promoting safe and efficient traffic operations within the site and at its access points. All driveways shall meet the design and construction standards of the City. Access to the site shall be designed to minimize conflicts with traffic on adjacent streets, particularly left turns into and from the site. For uses having frontage and/or access on a major traffic route, as defined in the City of Boyne City Comprehensive Plan, the number, design, and location of access driveways and other provisions for vehicular circulation shall comply with the access management provisions of this Ordinance.</p>	<p>Existing driveways on Lake street will be removed and replaced with sidewalk and on street parking. One driveway on State street will be eliminated, and on street parking developed. A 20' curbed driveway will be installed providing access to the site from State St. Proposed plan meets the access management provisions.</p>	
<p><u>F. Emergency Vehicle Access.</u> All buildings or groups of buildings shall be arranged so as to permit necessary emergency vehicle access as required by the Fire Department, Ambulance Department and Police Department.</p>	<p>Site accessibility has been reviewed by BCPD, Fire Dept and Ambulance Dept, necessary access for emergency vehicles is provided.</p>	
<p><u>G. Sidewalks, Pedestrian and Bicycle Circulation.</u> The arrangement of public or common ways for vehicular and pedestrian circulation shall be connected to existing or planned streets and sidewalks/pedestrian or bicycle pathways in the area. There shall be provided a pedestrian circulation system which is separated from the vehicular circulation system. In order to ensure public safety, special pedestrian measures, such as crosswalks, crossing signals and other such facilities may be required in the vicinity of primary and secondary schools, playgrounds, local shopping areas, fast food/service restaurants and other uses which generate a considerable amount of pedestrian or bicycle traffic.</p>	<p>Existing sidewalk on Lake Street and State street will be rebuilt and widened. Parking lot separated from sidewalk.</p>	
<p><u>H. Barrier-Free Access.</u> The site has been designed to provide barrier-free parking and pedestrian circulation.</p>	<p>Building has barrier free access.</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
LOFTS ON LAKE STREET**

<p><b>I. Parking.</b> The number and dimensions of off-street parking [spaces] shall be sufficient to meet the minimum required by this Ordinance. However, where warranted by overlapping or shared parking arrangements, the Planning Commission may reduce the required number of parking spaces, as provided in this Ordinance.</p>	<p>A 69 space off street parking lot is provided and 13 on street public parking spaces provided for total of 82. Spaces per ordinance is 118. Planning Commission may grant reductions as per 24.20(F) and/or 10.50(M). Parking study demand matrix shows project increases surplus parking for block 4 from 35 to 43, parking review for project provided by Cambourne Consulting, Over 300 public spaces with 2 min walk, 150 public spaces within 2 blocks at 23.6% average daily usage.</p>	
<p><b>J. Loading.</b> All loading and unloading areas and outside storage areas, including refuse storage stations, shall be screened in accordance with this Ordinance.</p>	<p>Dumpster location shown on development plan, masonry enclosure to match building proposed.</p>	
<p><b>K. Landscaping, Screening, and Open Space.</b> The landscape shall be preserved in its natural state, insofar as practical, by removing only those areas of vegetation or making those alterations to the topography which are reasonably necessary to develop the site in accordance with the requirements of this Ordinance. Landscaping shall be preserved and/or provided to ensure that proposed uses will be adequately buffered from one another and from surrounding public and private property. Landscaping, landscape buffers, greenbelts, fencing, walls and other protective barriers shall be provided and designed in accordance with the landscaping provisions of this Ordinance. Recreation and open space areas shall be provided in all multiple-family residential and educational developments.</p>	<p>Landscaping plan provided.</p>	
<p><b>L. Soil Erosion Control.</b> The site shall have adequate lateral support so as to ensure that there will be no erosion of soil or other material. The final determination as to adequacy of, or need for, lateral support shall be made by the Planning Director or City Engineer, and have a valid Charlevoix County Soil Erosion permit.</p>	<p>Soil erosion permit must be obtained if required.</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
LOFTS ON LAKE STREET**

<p><u>M. Stormwater Management.</u> Appropriate measures shall be taken to ensure that removal of surface waters will not adversely affect neighboring properties or the public storm drainage system. Provisions shall be made to accommodate stormwater which complements the natural drainage patterns and wetlands, prevent erosion and the formation of dust. Sharing of stormwater facilities with adjacent properties shall be encouraged. The use of detention/retention ponds may be required. Surface water on all paved areas shall be collected at intervals so that it will not obstruct the flow of vehicular or pedestrian traffic or create standing water. All such measures shall comply with the Charlevoix County Stormwater Ordinance.</p>	<p>Storm water plan provided.</p>	
<p><u>O. Lighting.</u> Exterior lighting shall be arranged so that it is directed preferably downward onto the subject site and deflected away from adjacent properties. Lighting shall not impede the vision of traffic along adjacent streets. Flashing or intermittent lights shall not be permitted.</p>	<p>Lighting plan and fixture spec sheets provided. Street lights matching existing City fixture to be installed on Lake and State</p>	
<p><u>P. Noise.</u> The site has been designed, buildings so arranged, and activities/equipment programmed to minimize the emission of noise, particularly for sites adjacent to residential districts.</p>	<p>Proposed uses are anticipated to produce noise that is typical of the CBD district. Site is not adjacent to any residential district.</p>	
<p><u>Q. Mechanical Equipment.</u> Mechanical equipment, both roof and ground mounted, shall be screened in accordance with the requirements of this Ordinance.</p>	<p>Mechanical equipment located on residential floors and roof.</p>	
<p><u>R. Signs.</u> The standards of the City of Boyne City's Sign Ordinance are met.</p>	<p>No signage shown on plan. Sign permit required for all proposed signage.</p>	
<p><u>S Hazardous Materials or Waste.</u> For businesses utilizing, storing or handling hazardous material such as automobile service and automobile repair stations, dry cleaning plants, metal plating industries, and other industrial uses, documentation of compliance with state and federal requirements shall be provided.</p>	<p>No hazardous materials or waste</p>	

**DEVELOPMENT SITE PLAN REVIEW STANDARDS  
FINDINGS OF FACT  
LOFTS ON LAKE STREET**

<p><u>T. Other Agency Reviews.</u> The applicant has provided documentation of compliance with other appropriate agency review standards, including, but not limited to, the Michigan Department of Natural Resources, Michigan Department of Environmental Quality, Michigan Department of Transportation, Charlevoix County Drain Commissioner, Northwest Michigan Community Health Agency, Charlevoix County Building Department, and other federal and state agencies, as applicable.</p>	<p>County permits and City W/WW and DPW approval required.</p>	
<p><u>U. Approval Process.</u> The development plan shall be reviewed by the Planning Commission. If disapproval is recommended, the Planning Commission shall cite reasons for such disapproval. If the Planning Commission finds a development plan not in conformity with this section, it may, at its discretion, return the development plan to the applicant with a written statement of the modifications necessary to obtain approval. Upon resubmission of the modified development plan, the Planning Commission shall review the plan. The Commission may approve, disapprove or approve subject to compliance with such modifications and conditions as may be deemed necessary to carry out the purpose of this Ordinance and other ordinances and resolutions of the City. If disapproved, the Planning Commission shall cite reasons for such disapproval.</p>		

## LOFTS ON LAKE STREET CBD DEVELOPMENT REQUIREMENTS FINDINGS

**Section 10.50 Development Requirements.** Buildings in the CBD should possess architectural variety, but must enhance the overall cohesiveness of the downtown's character and appearance as determined by the Planning Commission. Building heights, story levels, window sizes and proportions, architectural features, and building materials must remain consistent with those of existing or adjacent buildings within the downtown.

ORDINANCE REQUIREMENT	FACTS	FINDING
A. Development Plan. Development plan approval for all uses as specified in Article XIX of this Ordinance. The Planning Commission may also request a preliminary or conceptual review by the Downtown Development Authority or its Architectural Review Committee for development plans within the CBD.	Development Plan Provided, plan review completed by design committee.	
B. Building Placement. Buildings shall be built at lot lines with no setbacks, or the average setback of other buildings on the block as determined by the Planning Commission. The Commission may require greater setbacks if such space, in their determination, is needed for off-street parking or other requirements.	Building has approximately 5' setback from west lot line 0' on north and south and 4' on east	
C. Building Height. New buildings must contain at least two (2) stories if the building is proposed for a corner lot or is adjacent to a multiple story building, unless the Planning Commission determines requiring a second story will not significantly enhance the character and appearance of the downtown	3 Story building proposed on corner lot.	
D. Building Mass. Buildings located at gateways entering the CBD Central Business District shall mark the transition into and out of the downtown in a distinct fashion, using massing, additional height, contrasting materials and architectural embellishments to obtain this effect. Buildings on corner lots shall be considered more significant structures, since they have at least two (2) front façades visibly exposed to the street. The Planning Commission may require additional height and architectural embellishments, such as corner towers, relating to their location.	Building located on a corner lot on northern gateway of CBD district with two façades visible from street. Contrasting materials and architectural embellishments incorporated into façade.	
E. Façade Design. All visible building façades from public right-of-way or public land shall conform with the following design criteria: 1. Architectural Features. Building façades greater than thirty-three (33) feet in length shall contain architectural features, details and ornaments that are consistent with predominating architectural styles found within the downtown such as: arches; roof cornices; contrasting bases; contrasting masonry courses, or molding; pilasters or columns; corbeling; contrasting bands or color; stone or ceramic accent tiles; colonnades; or porches. Elements such as wall clocks, decorative light fixtures, and door or window canopies are recommended. Blank, windowless walls are prohibited. All non-residential buildings must have interior downspout and gutter systems; exterior downspouts and gutters are not permitted for non-residential buildings, except for those originally constructed for single-family residential purposes. 2. Fenestration. All façades visible from the street must contain glazed glass windows. Windows shall be recessed and include visually obvious sills. Spaces between windows shall be formed by columns, mullions, or material found elsewhere on the façade. Clear window glass is recommended; green, blue, bronze, or smoke tints are permitted. Window shapes shall be rectangular, square, or palladian (mostly rectangular with semi-circular top)		

**LOFTS ON LAKE STREET  
CBD DEVELOPMENT REQUIREMENTS FINDINGS**

<p>E(2)(a) Glazing on the first floor shall occupy a minimum sixty percent (60%) and a maximum of seventy percent (70%) of the façade. No glazing on first floor shall be placed less than two (2) feet six (6) inches above the sidewalk. No glazing on the first floor shall be placed more than eight (8) feet above sidewalk.</p> <p>E(2)(b) Glazing on the second or higher floors shall be a minimum thirty percent (30%) and a maximum of sixty percent (60%) of the façade. Vertical window orientation shall have a width-to-height ratio of at least one (1) to (2), and shall be consistent with adjacent buildings. Horizontal windows with a width-to-height ratio of between one (1) to one (1) and four (4) to one (1) may be permitted by the Planning Commission if they determine such window orientation is consistent with the appearance and character of the downtown. Shutters, if used, shall be mounted on either side of a window shall be equal to one-half (2) of the width and one (1) times the height.</p>	<p>Building façade greater than 33' building facades include contrasting architectural features and masonry. Rectangular windows, West side glazing 1st floor 62%, 2<sup>nd</sup> and 3<sup>rd</sup> floor 51%. North Side Glazing 1st floor 64%, 2<sup>nd</sup> and 3<sup>rd</sup> floors 45%.</p>	
<p>E(3). Building Materials. Building materials must be consistent with the surrounding neighborhood character, as determined by the Planning Commission. Building materials on the front façade or any façade visible from a public right-of-way must be primarily of natural materials conveying permanence, as determined by the Planning Commission. Each front façade, any façade visible from a public right-of-way, and any façade with a dedicated public entrance into the building should contain at least sixty percent (60%) percent of the recommended materials.</p>	<p>Building façades with dedicated public entrance exceed 60% of recommended materials.</p>	
<p>E(4). Exterior Colors. Exterior colors shall be compatible with the colors on adjacent buildings, subject to review and approval by the Planning Commission. The following natural colors are encouraged for the main portions of building façades and roof forms: neutral earth tones (sand to brown); shades of gray; traditional colors (e.g., brick red, forest green, navy blue); light, subdued hues (e.g., salmon); or white. Contrasting, accent colors which are compatible with the primary colors listed above are encouraged for trim, accent, and other decorative architectural features. The use of bright or fluorescent colors (e.g., purple, orange, pink, lime, yellow) is discouraged. Colors should be natural to the material or pigmented, and not painted on the material whenever possible.</p>	<p>Building colors for main portion of the building primarily consist of neutral earth tones</p>	
<p>F, Side or Rear Façade Design. All sides of a building shall be similar in design, detail, and material to present a cohesive appearance to neighboring properties. Wherever a side or rear façade is visible from a public right-of-way, or if parking is located at the side or rear of a building, the façade shall be designed to create a pleasing appearance. Materials and architectural features similar to those present on the front of the building shall be used on the side or rear façade. All visibly exposed sides of a building shall have an articulated base course and cornice. The base course shall align with either the kickplate or sill level of the first floor. The cornice shall terminate or cap the top of a building wall, and may project out horizontally from the vertical building wall plane and may be ornamented with moldings, brackets and other details. The middle section of a building may be horizontally divided at floor, lintel,</p>	<p>Side and rear façades not provided.</p>	

**LOFTS ON LAKE STREET  
CBD DEVELOPMENT REQUIREMENTS FINDINGS**

<p>or sill levels with belt or string courses. Waste receptacle and service areas shall be completely screened with a decorative masonry wall or some other acceptable form of screening as approved by the Planning Commission. Open areas shall be landscaped with lawn, ground cover, ornamental shrubs and trees. On every site involving new development or redevelopment, foundation plantings adjacent to the building shall be provided. The species and design shall meet the requirements of Article XXIII, Landscaping Requirements.</p>		
<p>G. <b>Building Entrances.</b> All buildings shall have at least one (1) primary public entrance that faces a public street unless a building does not face a public street and/or right-of-way. Rear entrances are permitted, only if there is a primary entrance from a public street. Main entrances to buildings shall incorporate devices such as canopies, recessed entrance ways, larger door openings and display windows, accent colors, and architectural details such as tile work, moldings, and distinctive door pulls. Doors measuring seven (7) and eight (8) feet high are highly recommended. Doors measuring six (6) feet eight (8) inches high shall have a glass transom with a minimum height of twelve (12) inches.</p>	<p>Public Entrances locations on Lake street. Doors measure between 7 and 8 feet.</p>	
<p>H. <b>Rooflines</b> shall be consistent with adjacent buildings and the surrounding neighborhood character as determined by the Planning Commission. Flat roofs shall be used in the CBD.</p>	<p>Flat roof proposed.</p>	
<p>I. Exterior lighting must be placed and shielded so as to direct the light onto the site and away from adjoining properties. The lighting source shall not be directly visible from adjoining properties. Floodlights, wall pack units, other types of unshielded lights, and lights where the lens is visible outside of the light fixture shall be prohibited, except where historic-style lighting is used that is compatible with existing historic-style lamps approved by the Planning Commission. Sidewalks and parking areas shall be properly lit to facilitate the safe movement of pedestrians and vehicles and provide a secure environment. In parking areas, the light intensity shall average a minimum of one (1) foot candle, measured five (5) feet above the surface. Parking lot lighting shall be consistent and/or similar with other fixtures used throughout the downtown, as determined by the Planning Commission, and no greater than twenty-four (24) feet. In pedestrian areas, the light intensity shall average a minimum of two (2) foot candles, measured five (5) feet above the surface.</p>	<p>Lighting plan and fixture spec sheets provided.</p>	
<p>J. <b>Canopies and Awnings.</b> Canopies and awnings shall be permitted on buildings as follows:  1. All awnings must be made from canvass fabric or similar water-proofed material, rather than metal, aluminum, plastic, vinyl, or rigid fiberglass.  2. All awnings shall be attached directly to the building, rather than supported by columns or poles.  3. In buildings with multiple storefronts, compatible awnings shall be used as a means of unifying the structure.</p>	<p>None Proposed</p>	
<p>K. <b>Signs.</b> Signs for all uses shall be permitted as specified in the Boyne City Sign Ordinance.</p>	<p>Signs details not provide, sign permit required.</p>	

**LOFTS ON LAKE STREET  
CBD DEVELOPMENT REQUIREMENTS FINDINGS**

<p>L Mechanical Equipment. All units and appliances for air conditioning, HVAC systems, exhaust pipes or stacks, elevator housing and satellite dishes or other telecommunications receiving devices shall be thoroughly screened from view from the public right-of-way and from adjacent properties, by using walls, fences, roofline elements, penthouse-type screening devices or landscaping. Fire escapes shall not be permitted on a building's front façade. In buildings requiring a second means of egress pursuant to the local building codes, internal stairs or other routes of egress shall be used. Solid metal security gates or solid roll-down metal windows shall be prohibited. Link or grill type security devices shall be permitted only if installed from inside, within the window or door frames; or if installed on the outside, if the coil box is recessed and concealed behind the building wall. Security grills shall be recessed and concealed during normal business hours. Models which provide a sense of transparency by colors, are encouraged. Other security devices fastened to the exterior walls are prohibited.</p>	<p>Mechanical equipment on each residential floor and east side of roof near elevator overrun</p>	
<p>M. Parking and Loading. The Planning Commission shall determine if the number of off-street parking and loading/unloading spaces required per Article XXIV of this Ordinance shall be met, or if a lesser number spaces or no spaces are required due to the following: the availability of on-street parking spaces, off-site parking lots, or municipal parking lots; a finding that patrons will either walk to the site from nearby neighborhoods, or will park at other sites and visit several uses at one time; or the placement and configuration of existing buildings. If required, parking lot design shall comply with the standards below, in addition to the provisions of Article XXIV.</p>	<p>118 spaces required by Article XXIV, 69 private off street and 13 public on street parking spaces provided.</p>	
<p>N. Service Access. A service alley or designated loading space shall be reserved at the rear of the building. Loading from secondary streets may be permitted by the Planning Commission upon demonstration by the applicant that through traffic flow and access to neighboring uses will not be disrupted.</p>	<p>Alley access from Park St provided.</p>	
<p>O. Landscaping shall comply with the following provisions and Article XXIII of this Ordinance.  1. On every site involving new development or redevelopment, street trees with a minimum caliper of two-and-a-half (2 2) inches shall be provided at twenty-five (25) foot intervals. Any of the following street trees with a minimum caliper of two-and-a-half (2 2) inches shall be planted within the road right-of-way at twenty-five (25) foot intervals: Red Maple, Green Ash, Bradford Pear, or Little Leaf Linden, White Ash, or Honey Locust, subject to review and approval by the Planning Commission.  2. On every site involving new development or redevelopment, a landscape plan shall be submitted for review and approval. The landscape design shall compliment the character of the downtown.  3. Lots for apartment and non-residential uses shall balance the functional requirements of parking with the provision of pedestrian amenities. Transition areas between parking and civic, commercial or residential uses shall be designed with textured paving, landscaping and street furniture.</p>	<p>Landscaping plan provided.</p>	

**LOFTS ON LAKE STREET  
CBD DEVELOPMENT REQUIREMENTS FINDINGS**

<p>P. <u>Screening</u>. Where a new or expanded use occurs in this District, after the effective date of this Zoning Ordinance, which new or expanded use abuts directly upon a residential district, protective screening shall be provided.</p>	<p style="text-align: center;">Does not abut residential district.</p>	
<p>Q. <u>Courtyards and Plazas</u>. Exterior public and semi-public spaces, such as courtyards or plazas, shall be designed for function, enhance surrounding buildings, and provide amenities for users in the form of textured paving, landscaping, lighting, trees, benches, trash receptacles and other items of street furniture, as appropriate. Courtyards shall have recognizable edges defined on at least three (3) sides by buildings, walls, elements of landscaping, and elements of street furniture, in order to create a strong sense of enclosure.</p>	<p style="text-align: center;">Building setback from west property line to creates increased pedestrian area in front of building.</p>	
<p>R. <u>Utilities</u>. All public and semi-public utilities and services, including but not limited to electricity, telephone, cable television, and others, shall be placed underground.</p>	<p style="text-align: center;">Utilities underground</p>	
<p>S. <u>Enclosed Buildings</u>. Within the CBD, all activities, unless specifically provided for herein, shall be conducted entirely within an enclosed building.</p>	<p style="text-align: center;">All activities within enclosed building</p>	



**BOYNE CITY**  
LAKE STREET DEVELOPMENT

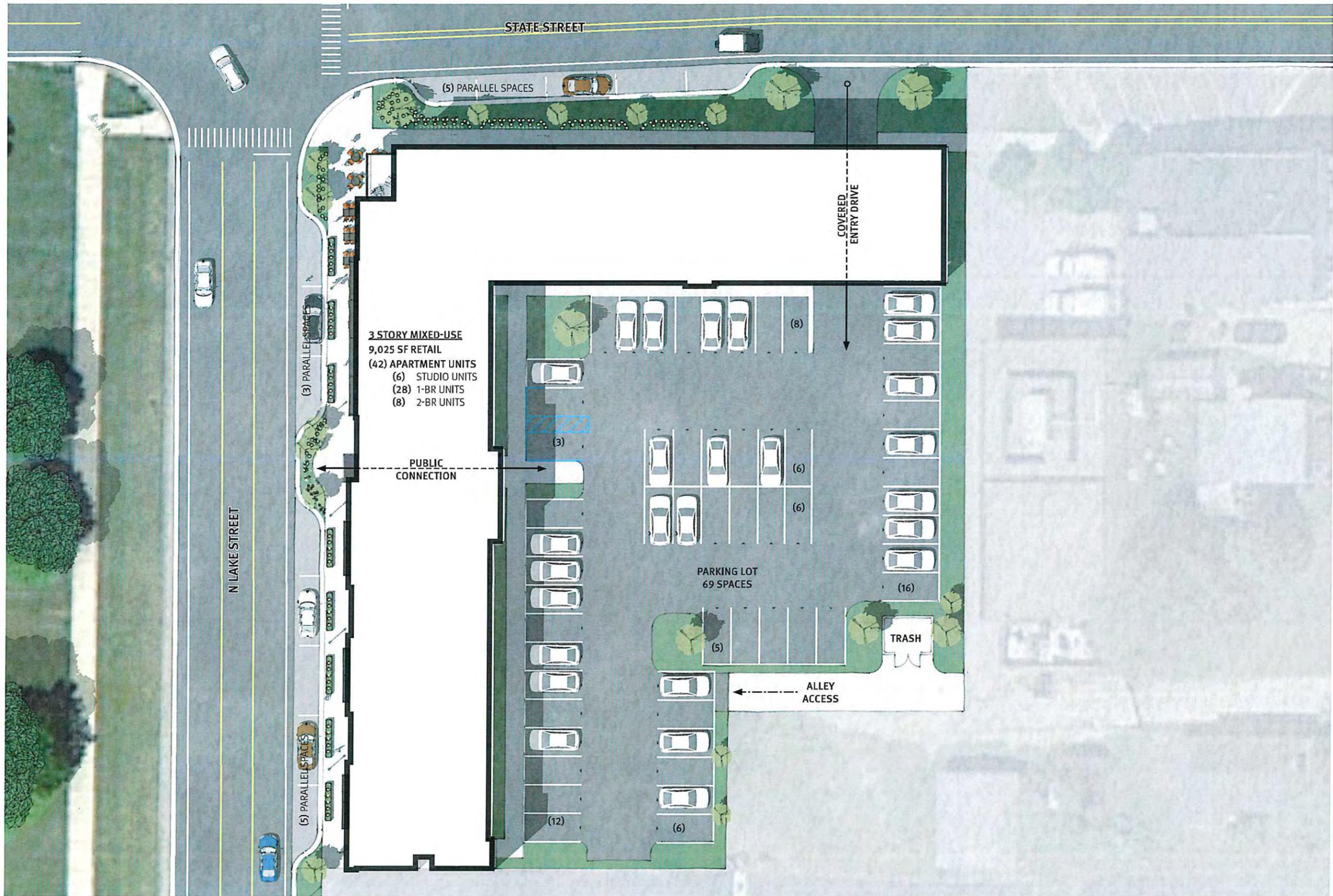
SITE PLAN SUBMITTAL  
**06 MAY 2019**

**T A B L E . O F . C O N T E N T S**

EXISTING SITE AERIAL	1
PROPOSED SITE PLAN	2
EXTERIOR IMAGES	3-4
PROPOSED EXTERIOR ELEVATIONS	5
ARCHIVE	6-16





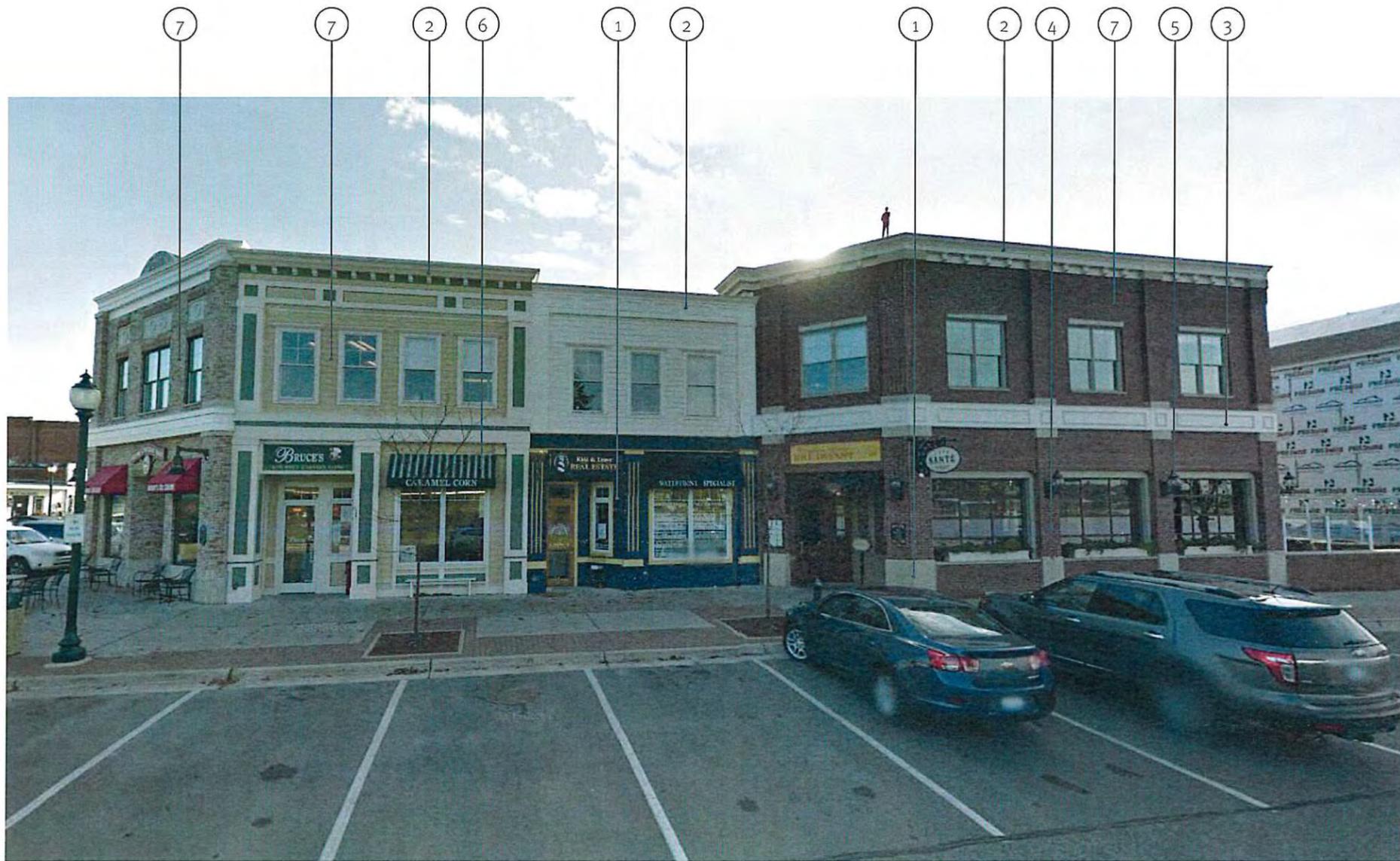








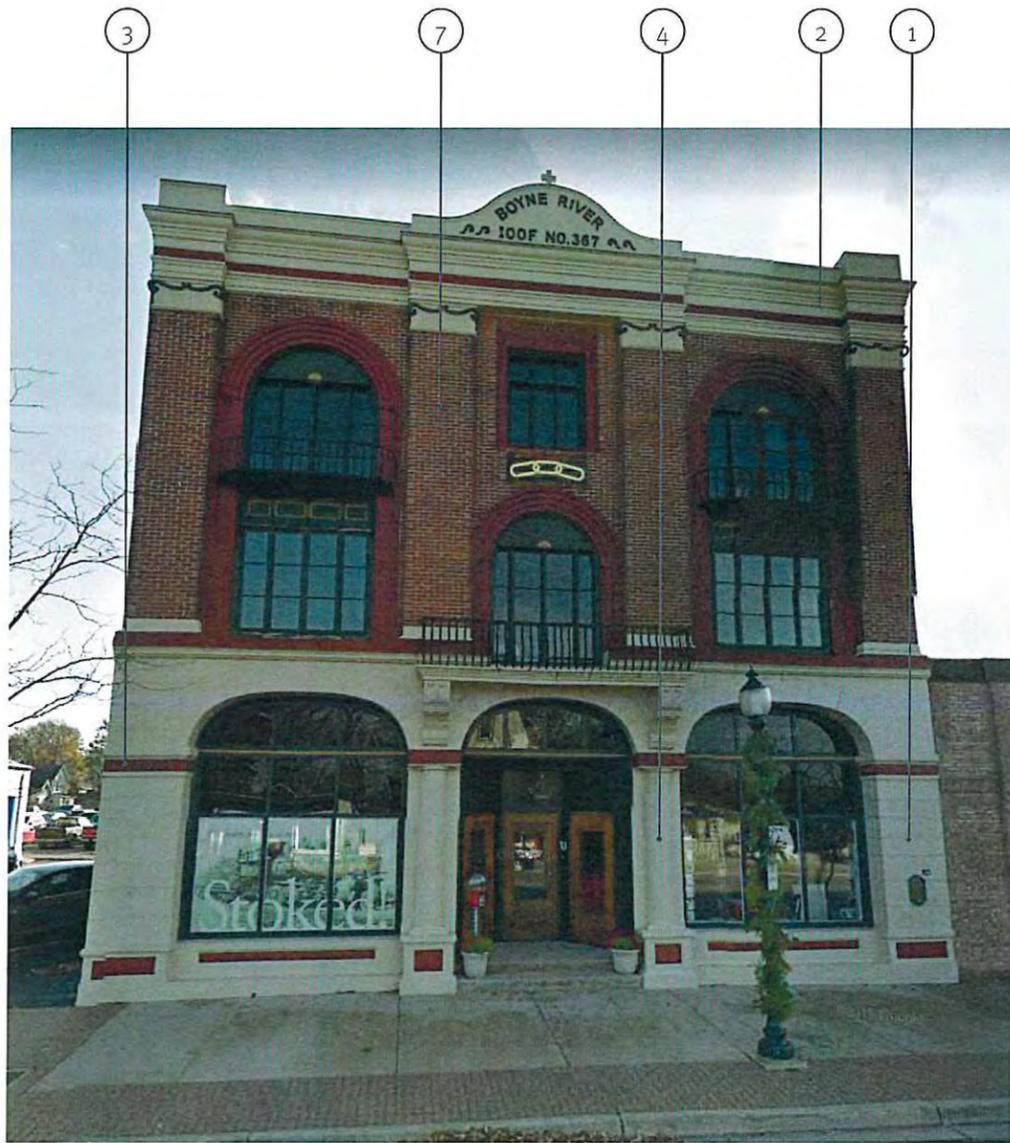
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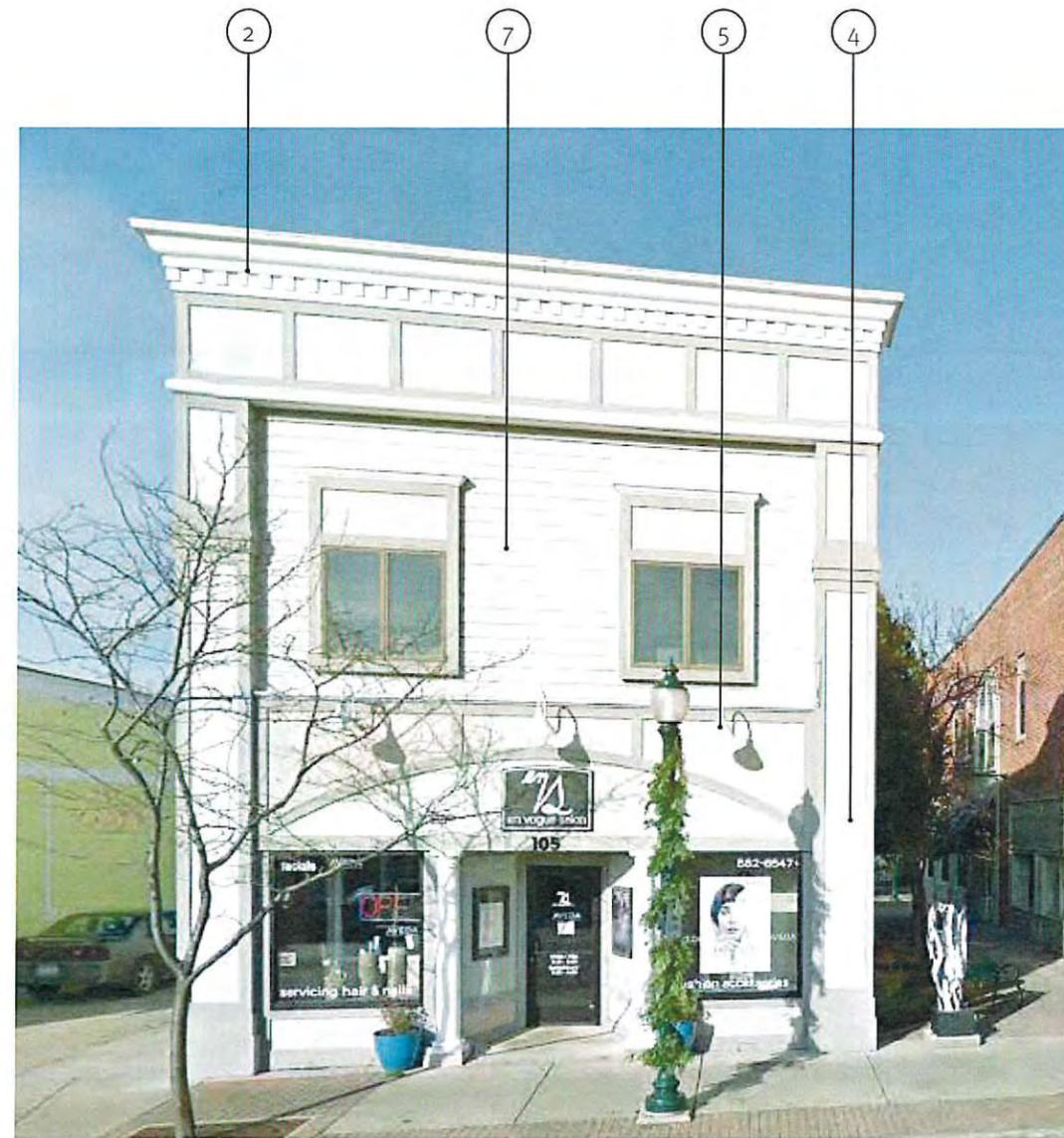
WATERS STREET

**BOYNE CITY EXISTING ARCHITECTURE  
DEVELOPMENT REQUIREMENTS**

- ① Contrasting base
- ② Roof cornice
- ③ Contrasting masonry courses
- ④ Columns / Collonade
- ⑤ Decorative light fixtures
- ⑥ Door / Window canopies
- ⑦ Brick / Stone / Wood facade material



WATERS STREET



WATERS STREET

**BOYNE CITY EXISTING ARCHITECTURE  
DEVELOPMENT REQUIREMENTS**

- ① Contrasting base
- ② Roof cornice
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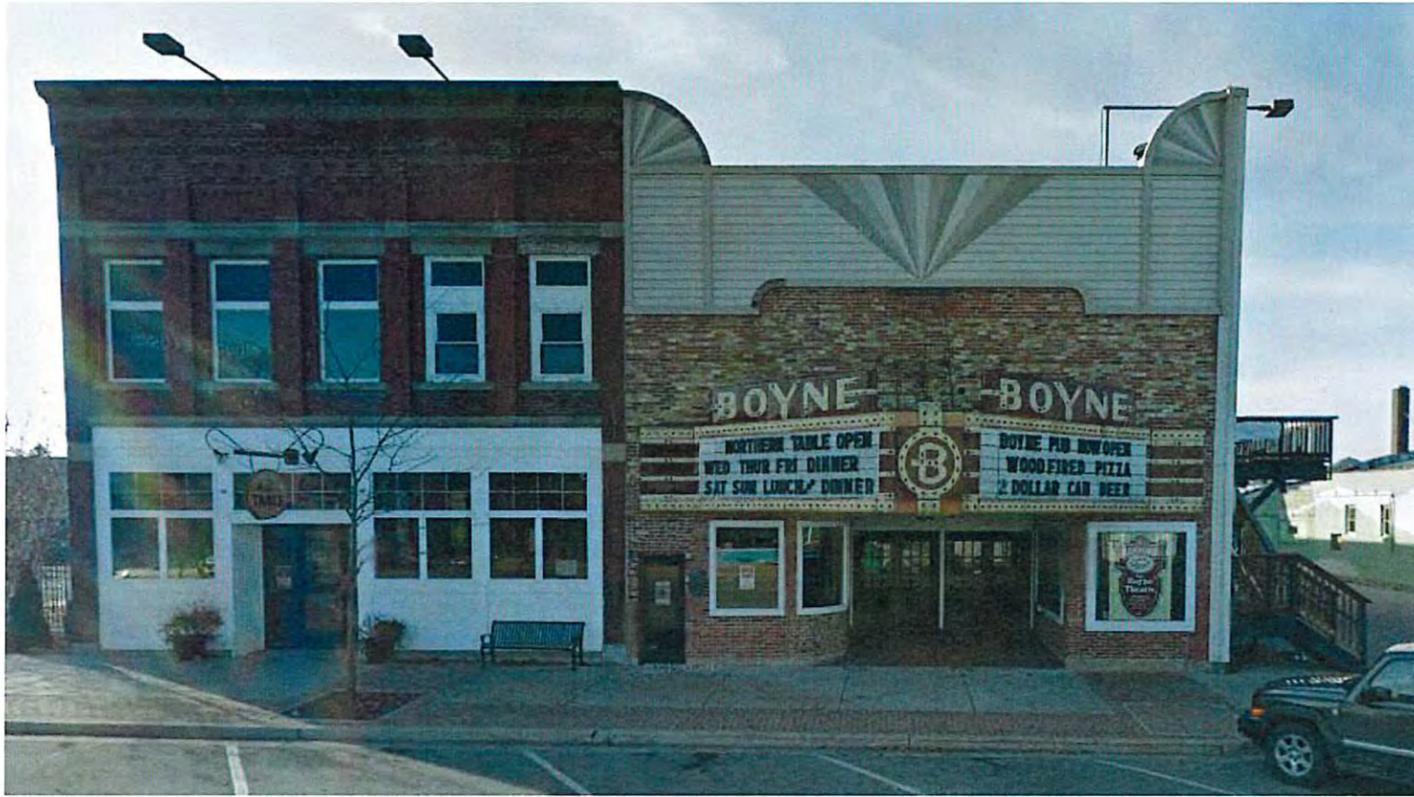
WATERS STREET



BOYNE AVENUE



WATERS STREET



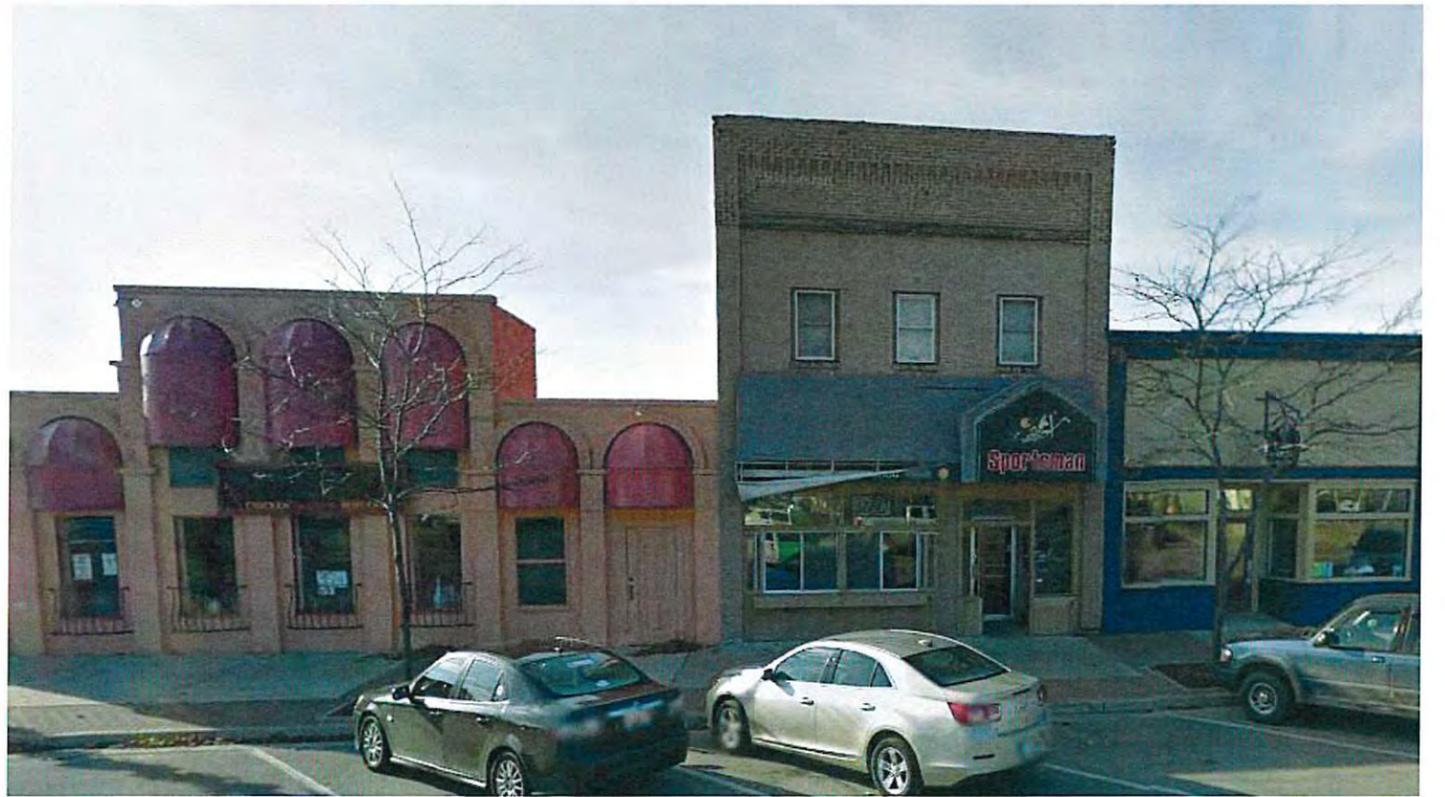
LAKE STREET



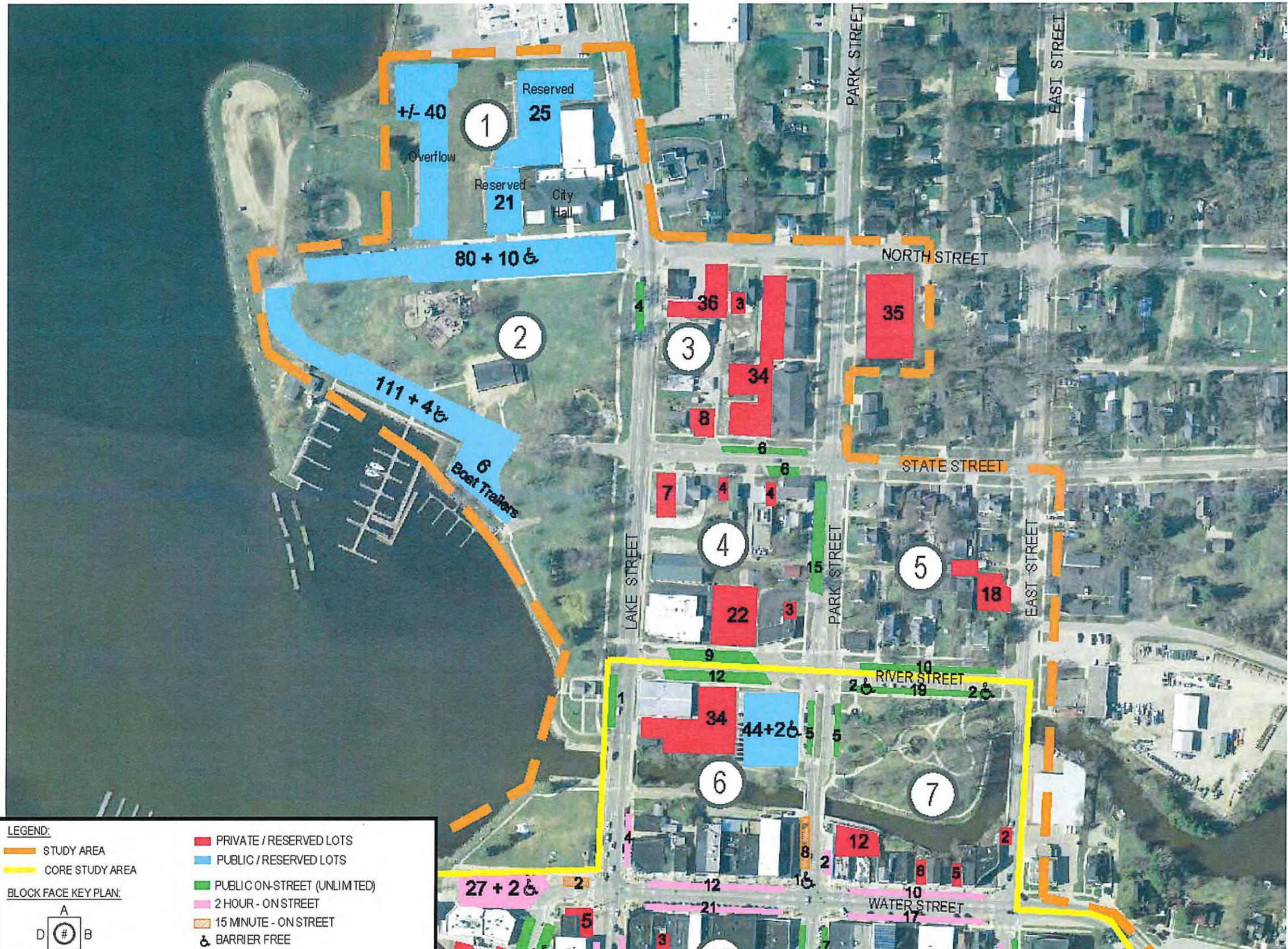
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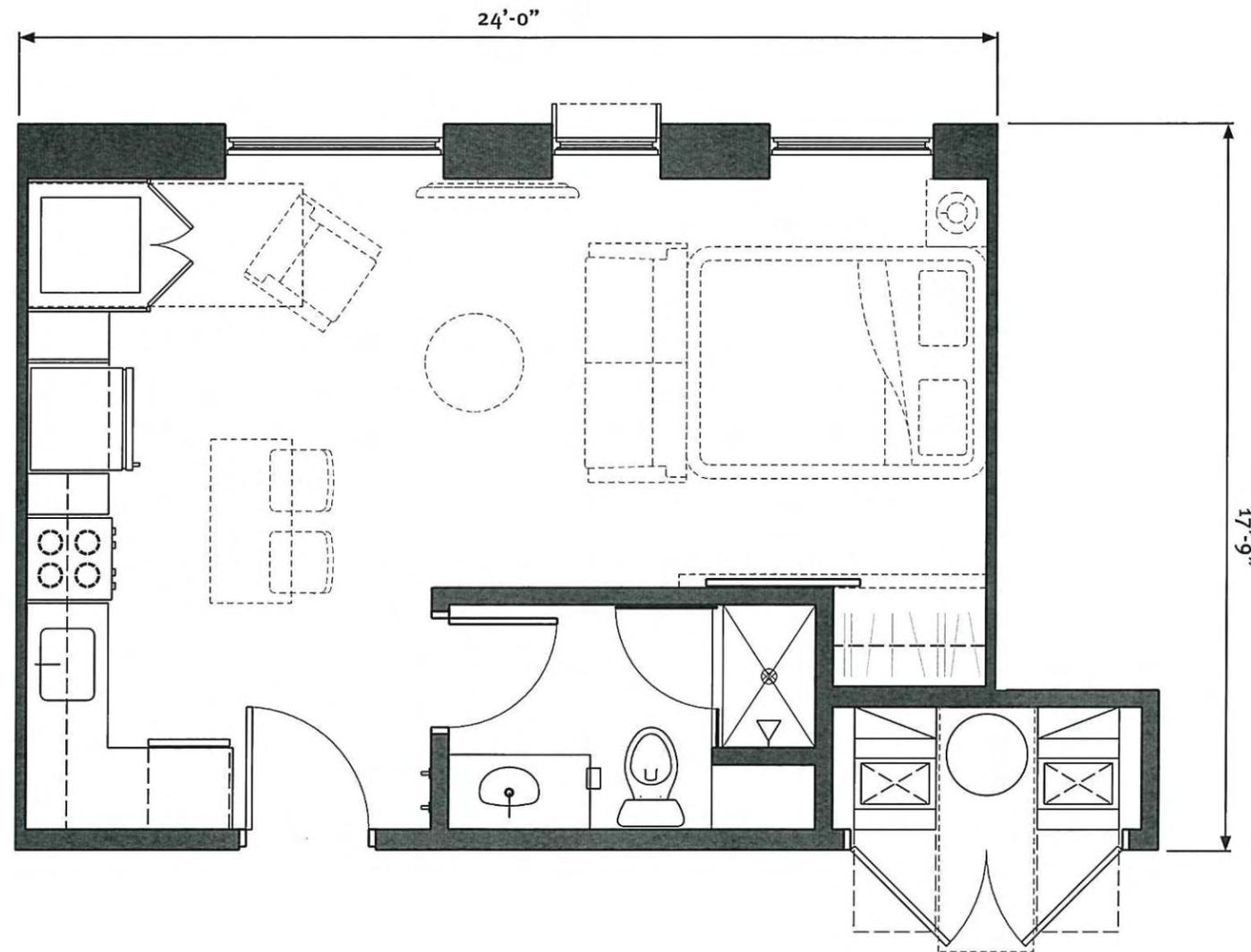
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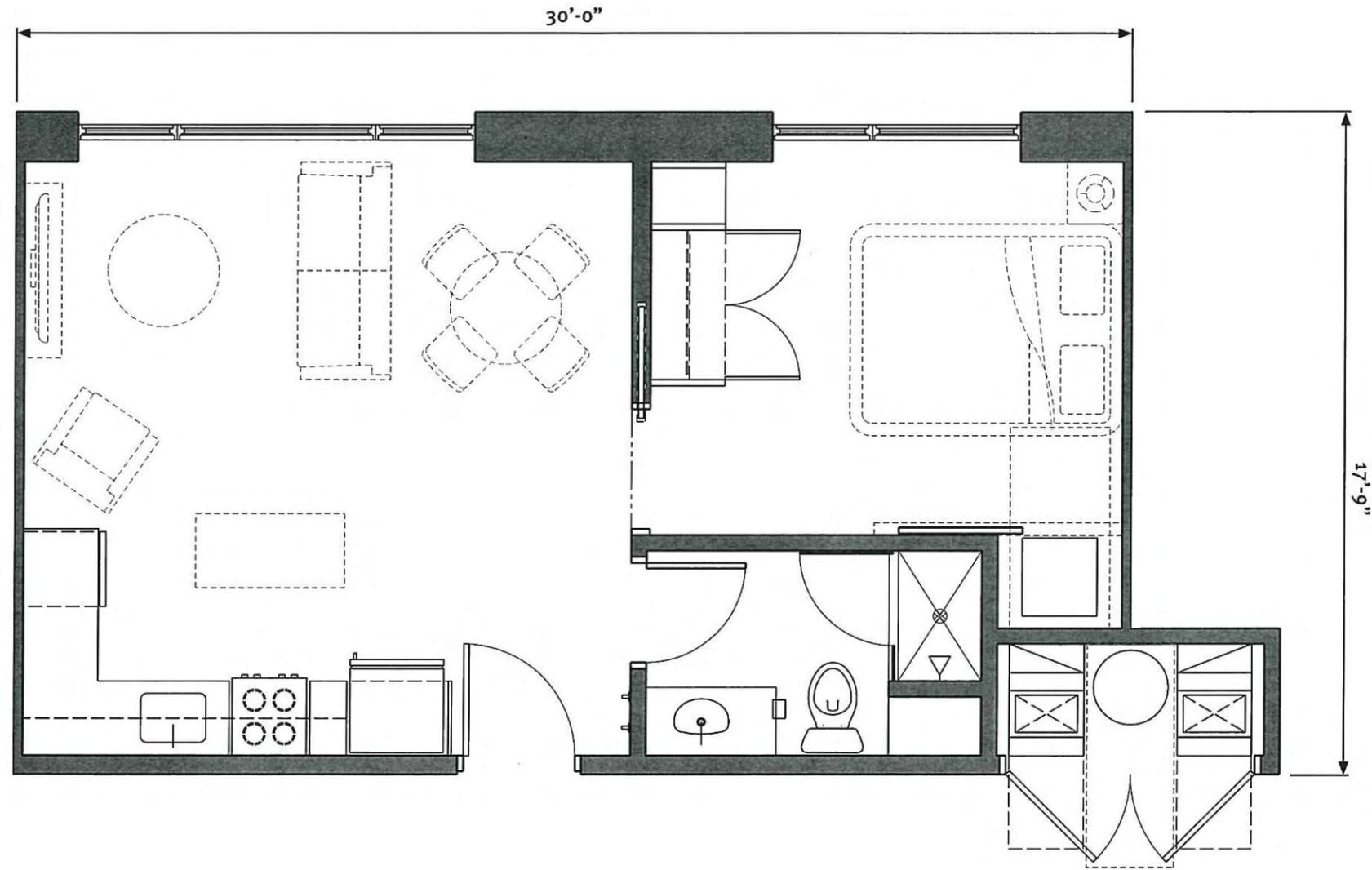


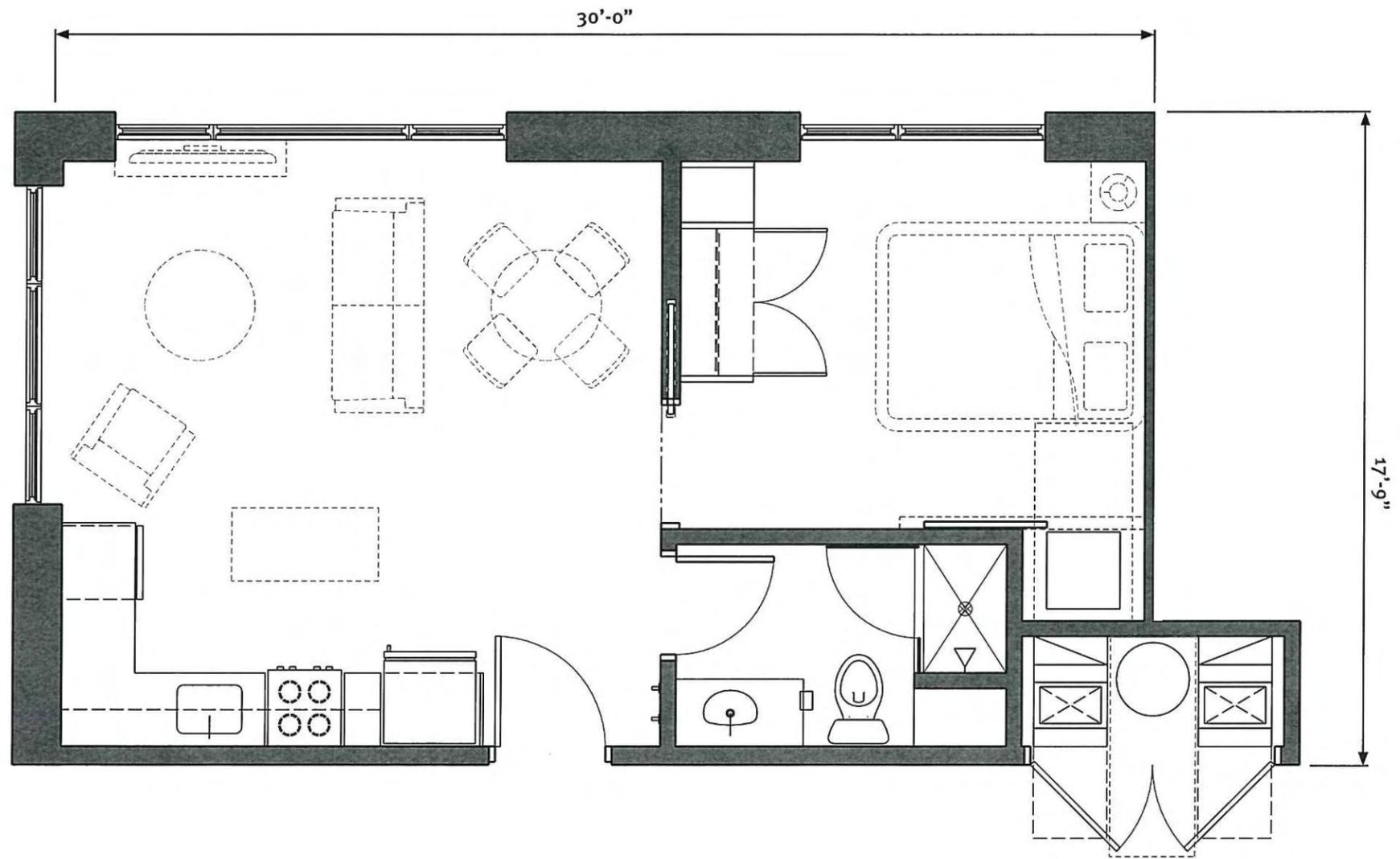
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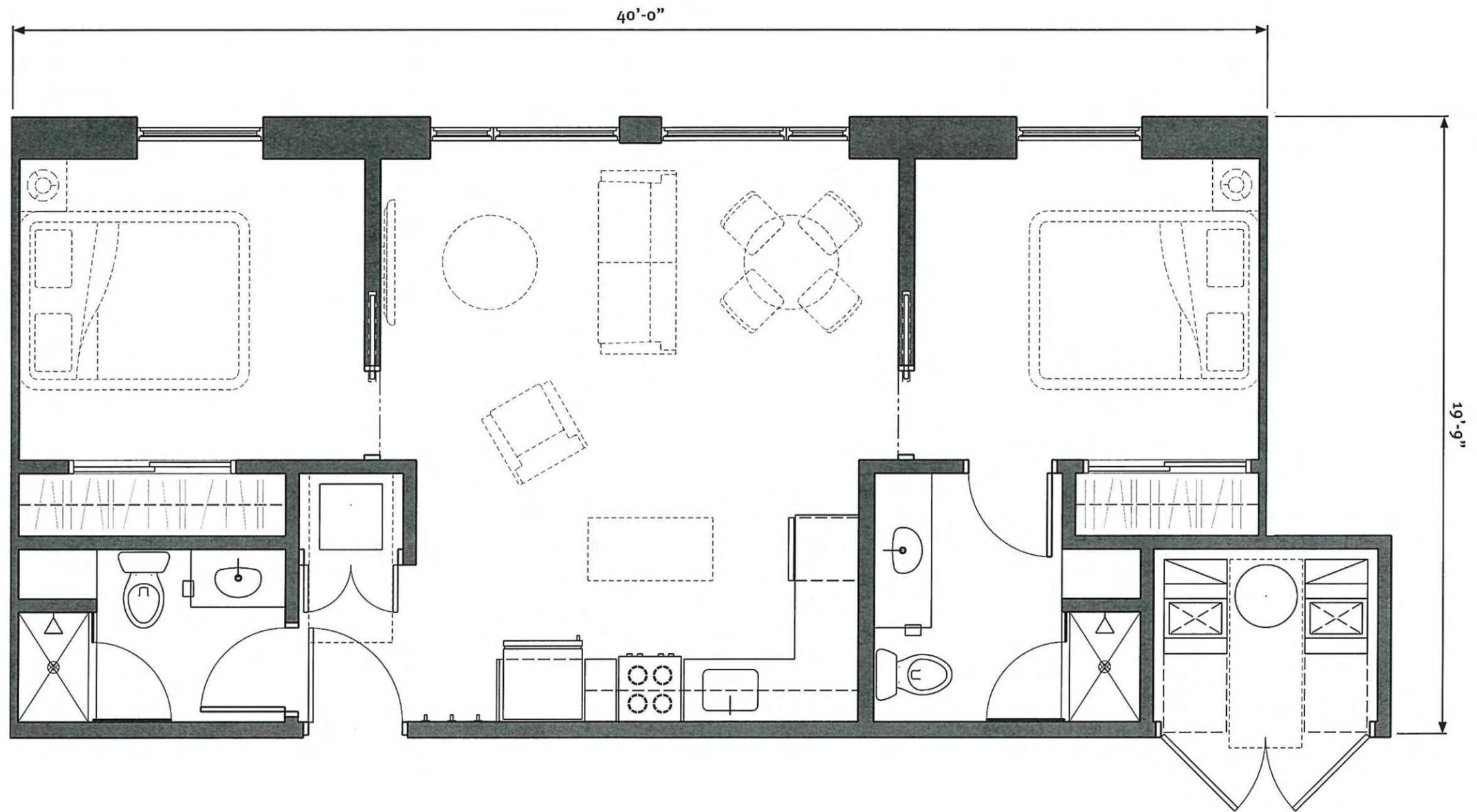


<p><b>CITY OF BOYNE PARKING STUDY</b></p> <p>Boyne City, Michigan</p>	<p><b>RICH &amp; ASSOCIATES PARKING CONSULTANTS</b></p> <p>2077 Southman Road, Suite 205 Bozette, Michigan 49312</p> <p>Southfield, MI 48034-1414 248-255-3240 800-949-8980</p> <p>ARCHITECTS • ENGINEERS • PLANNERS</p>	<p><b>LEGEND:</b></p> <ul style="list-style-type: none"> <li><span style="color: orange;">—</span> STUDY AREA</li> <li><span style="color: yellow;">—</span> CORE STUDY AREA</li> <li><span style="color: red;">■</span> PRIVATE / RESERVED LOTS</li> <li><span style="color: blue;">■</span> PUBLIC / RESERVED LOTS</li> <li><span style="color: green;">■</span> PUBLIC ON-STREET (UNLIMITED)</li> <li><span style="color: pink;">■</span> 2 HOUR - ON STREET</li> <li><span style="color: orange;">■</span> 15 MINUTE - ON STREET</li> <li><span style="color: blue;">♿</span> BARRIER FREE</li> </ul>
	<p><b>ARCHITECTS • ENGINEERS • PLANNERS</b></p> <p> <b>BOYNE</b></p> <p>09-14-18 sar</p>	<p><b>BLOCK FACE KEY PLAN:</b></p> <p>A D <span style="border: 1px solid black; padding: 2px;">#</span> B C</p>









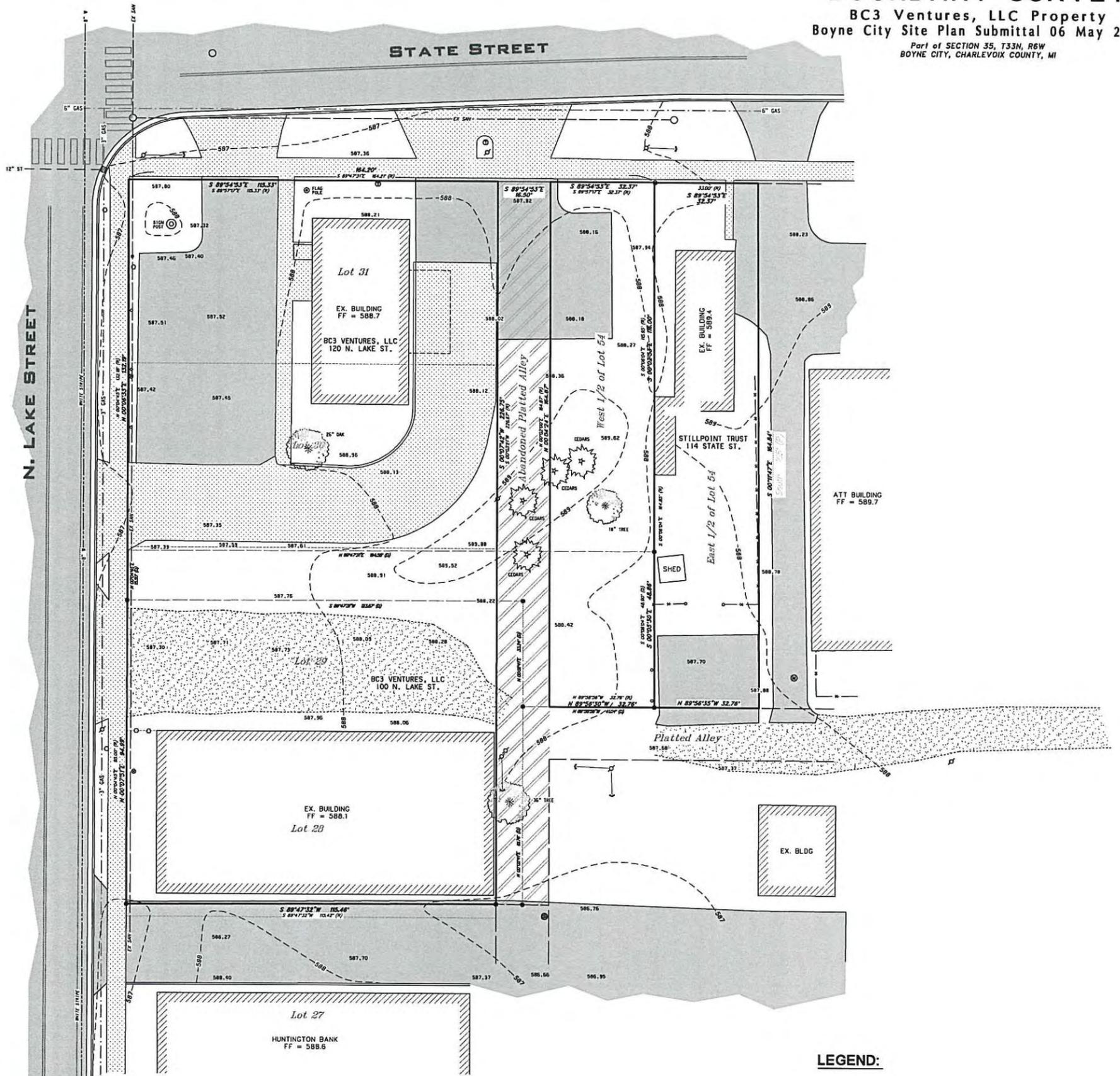


SCALE 1" = 16'

# TOPOGRAPHIC & BOUNDARY SURVEY

BC3 Ventures, LLC Property  
Boyer City Site Plan Submittal 06 May 2019  
Part of SECTION 35, T33N, R6W  
BOYER CITY, CHARLEVOIX COUNTY, MI

BENCH MARK  
TOP NJT on HYDRANT  
ELEV = 589.89 GPS DATUM



### LEGEND:

- = SIGN
- ⊕ = UTILITY POLE w/ GUY WIRE
- ⊕ = TELEPHONE PEDESTAL
- = CURBSTOP
- = POST
- ⊕ = FIRE HYDRANT
- ⊕ = STORM SEWER CATCH BASIN
- ⊕ = MANHOLE COVER
- ⊕ = MONITOR WELL
- = PROPERTY LINE
- = FENCE
- = APPROXIMATE WATERMAIN
- = APPROXIMATE SANITARY SEWER
- = APPROXIMATE STORM SEWER
- = FOUND SURVEY IRON, AS DESCRIBED
- ⊕ = FOUND SURVEY DEMARCATION AS DESCRIBED
- 588.03 = EX. SPOT ELEVATION
- ▨ = EX. CONCRETE SURFACE
- ▨ = EX. ASPHALT SURFACE
- ▨ = EX. GRAVEL SURFACE

### NOTES:

UTILITIES LINES SHOWN SHALL BE CONSIDERED APPROXIMATE LOCATION.  
WATER & SEWER UTILITY LINES SHOWN WERE TAKEN FROM DATA PROVIDED  
BY THE CITY DPW.  
CALL MISS DIG THREE (3) FULL WORK DAYS PRIOR TO EXCAVATION.



3 WORKING DAYS 1  
BEFORE YOU DIG  
CALL MISS DIG  
800-482-7171

**BENCHMARK ENGINEERING INC.**  
SURVEYORS • CIVIL ENGINEERS  
807 E. LAKE ST.  
HARBOR SPRINGS, MICHIGAN 49710  
PHONE (231) 526-2109 FAX (231) 526-7257  
benchmark1607@gmail.com

Client: INTEGRATED ARCHITECTURE  
Project Mgr: B. NOLAN  
Drawn By: B. NOLAN  
Field By: WINTER-WILSON  
CAD File: EX 19-031 06 May 19.gxd  
Job #: 19-031  
Sheet #: 1 OF 3

Date: (revisions) by
06 MAY 2019

**TOPOGRAPHIC & BOUNDARY SURVEY**  
BC3 Ventures - Boyer City Site Plan Submittal

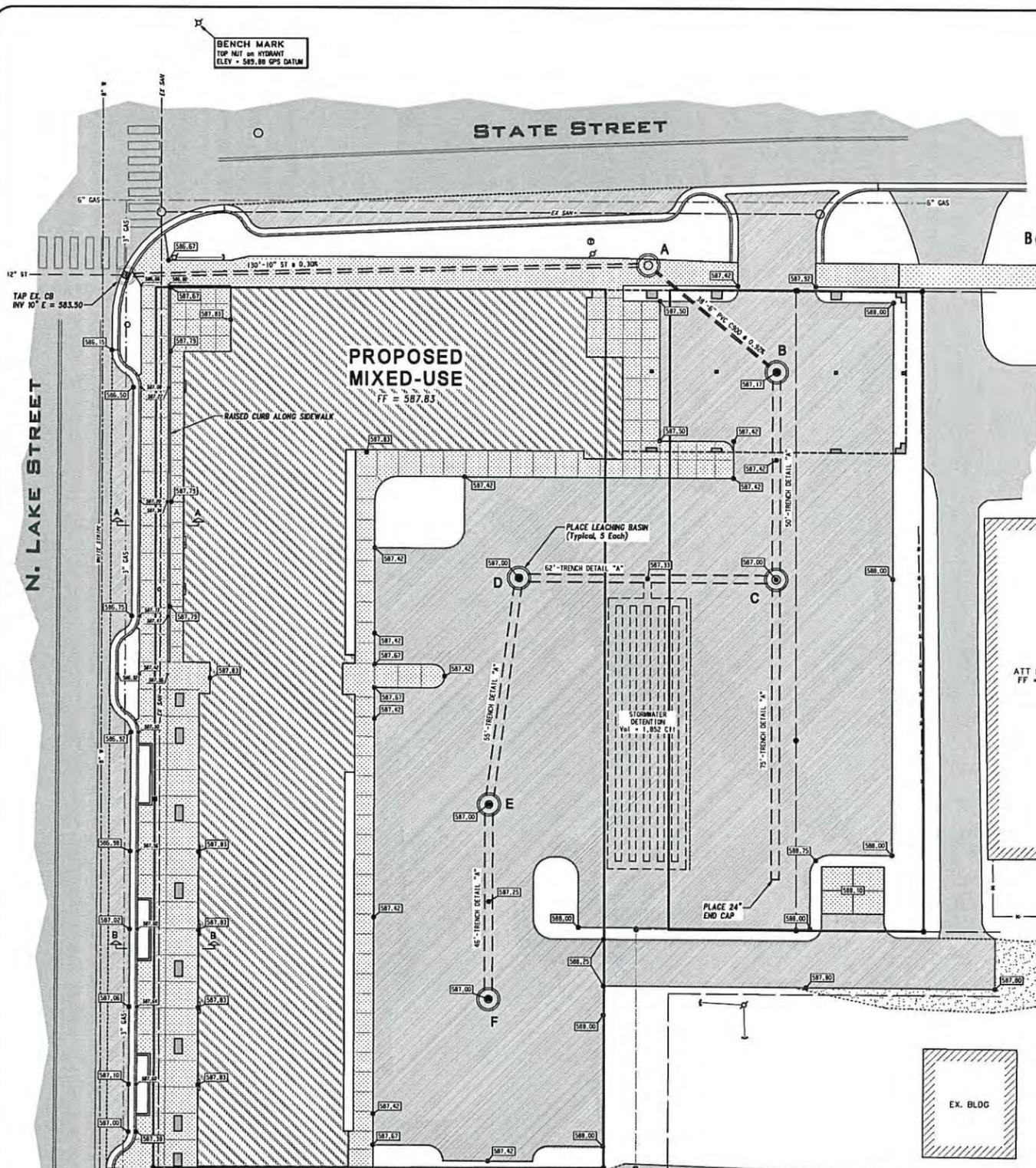
BENCH MARK  
TOP MET. HYDRANT  
ELEV. = 589.86 G.P.S. DATUM



SCALE 1" = 16'

# PROPOSED GRADING & DRAINAGE PLAN

BC3 Ventures, LLC Property  
Boyne City Site Plan Submittal 06 May 2019  
Part of SECTION 35, T33N, R6W  
BOYNE CITY, CHARLEVOIX COUNTY, MI



<b>STORM "A"</b>	4' DIA. LEACHING BASIN EJ 1040 FRAME, 8 COVER RIM ELEV. = 587.17 INV 6" x 6" = 583.30 INV 10" x 6" = 583.50
<b>STORM "B"</b>	4' DIA. LEACHING BASIN EJ 1130 FRAME, SOLID COVER RIM ELEV. = 587.17 INV 6" x 6" = 584.25 INV 24" x 6" = 583.75
<b>STORM "C"</b>	4' DIA. LEACHING BASIN EJ 1040 FRAME, MI COVER RIM ELEV. = 587.00 INV 24" x 6" x 5' = 583.75
<b>STORM "D"</b>	4' DIA. LEACHING BASIN EJ 1040 FRAME, MI COVER RIM ELEV. = 587.00 INV 24" x 6" x 5' = 583.75
<b>STORM "E"</b>	4' DIA. LEACHING BASIN EJ 1040 FRAME, MI COVER RIM ELEV. = 587.00 INV 24" x 6" x 5' = 583.75
<b>STORM "F"</b>	4' DIA. LEACHING BASIN EJ 1040 FRAME, MI COVER RIM ELEV. = 587.00 INV 24" x 6" x 5' = 583.75

### DRAINAGE CALCULATIONS

25 Yr. 24 Hr STORM (1) = 3.5"  
EX. 50 L/S: USDA Au Grcs (AuA) - SANDY  
ASSUMED PERCOLATION RATE = 12"/24 HR  
PAVEMENT RUN-OFF COEFFICIENT (C) = 0.9

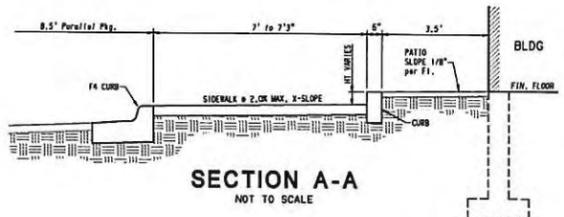
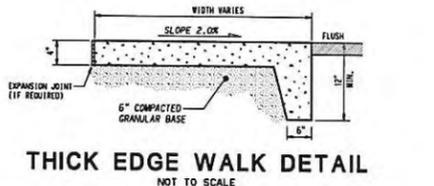
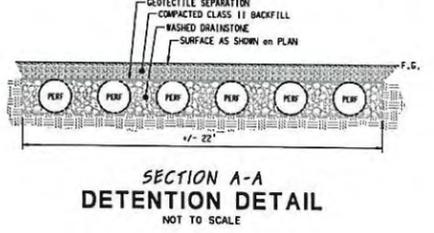
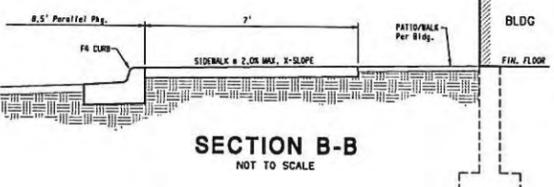
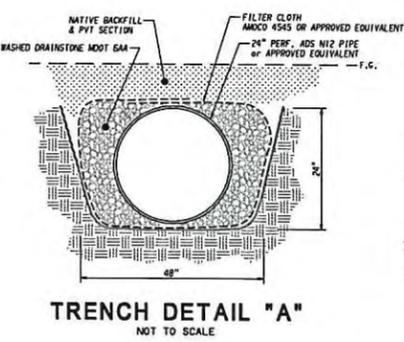
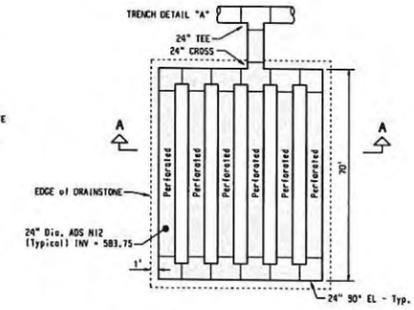
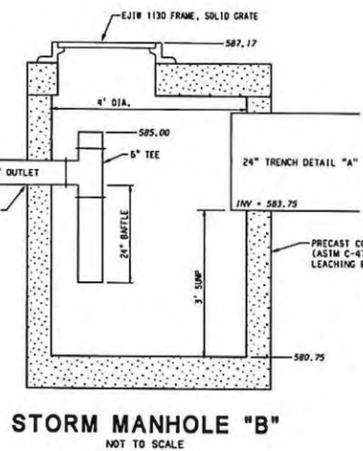
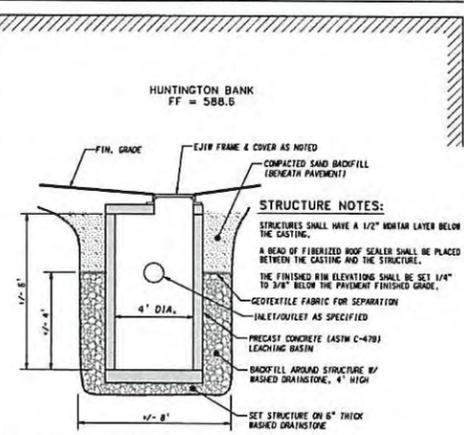
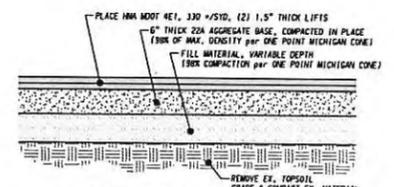
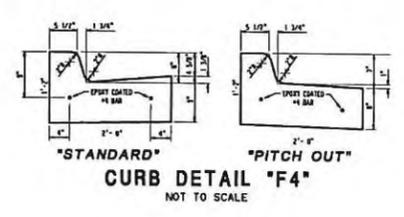
**SITE CONDITIONS:**  
EX. HARD SURFACE (A1) = 26,000 SII  
PROP. HARD SURFACE (A2) = 40,600 SII  
NET ADD'L. HARD SURFACE (A3) = 14,600 SII

**CALCULATED RETENTION VOLUME: (V1)**  
VOL. =  $C \times I \times A$   
= 0.9 x 3.5/12 x 14,600 SII  
= 3,832 CFI

**RETENTION VOLUME PROVIDED:**  
(5) LEACHING BASINS = 1,050 CFI  
STORMWATER DETENTION = 1,852 CFI  
(288 L) TRENCH DRAIN = 1,325 CFI  
TOTAL RETENTION Vol. = 4,227 CFI

**PRE-DEVELOPMENT PEAK 2 Yr DISCHARGE:**  
 $Q = C \times I \times A$   
( $T_c = 15 \text{ min}, c = 0.90, i = 2.5, A1 = 0.50 \text{ Ac.}$ )  
 $Q = 0.9 \times 2.5 \times 0.60$   
 $Q = 1.35 \text{ cfs}$

6" DISCHARGE @ 1.0% SLOPE = 0.75 cfs



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- ⊙ = MANHOLE COVER
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- ⊙ = FOUND SURVEY DEMARCATION AS DESCRIBED
- 589.09 = EX. SPOT ELEVATION
- ▨ = EX. CONCRETE SURFACE
- ▨ = EX. ASPHALT SURFACE
- ▨ = EX. GRAVEL SURFACE
- ▨ = PROPOSED ASPHALT SURFACE
- ▨ = PROPOSED CONCRETE SURFACE
- ▨ = PROPOSED GRADE

### NOTES:

UTILITIES LINES SHOWN SHALL BE CONSIDERED APPROXIMATE LOCATION. WATER & SEWER UTILITY LINES SHOWN WERE TAKEN FROM DATA PROVIDED BY THE CITY DPW.

CALL MISS DIG THREE (3) FULL WORK DAYS PRIOR TO EXCAVATION.

**BENCHMARK ENGINEERING INC.**  
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HARBOR SPONS, MICHIGAN 49740  
PHONE (231) 525-2778 FAX (231) 525-7257  
benchmark7@gmail.com

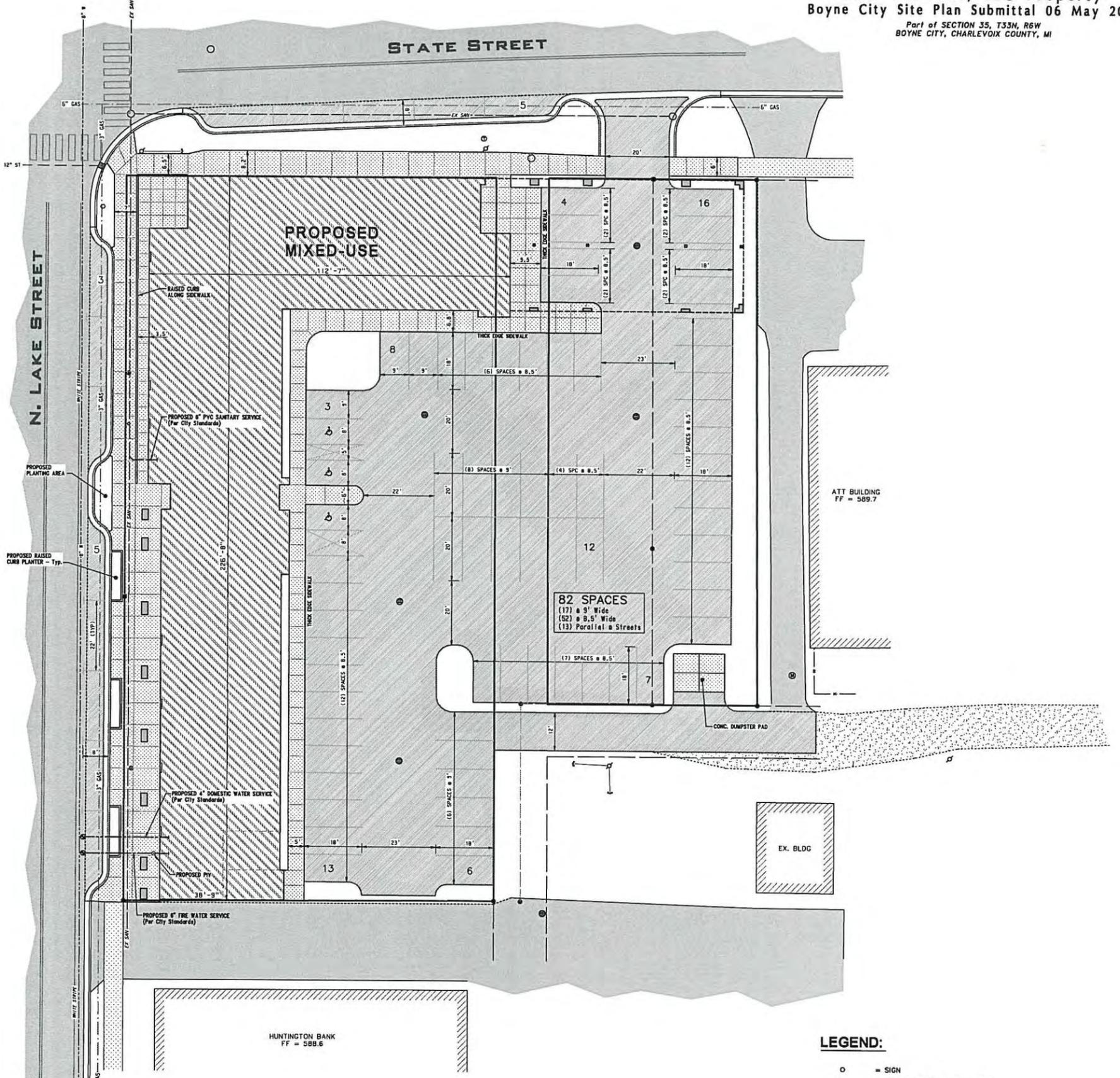
Client: INTEGRATED ARCHITECTURE	Date: (revisions) by
Project Mgr: B. NOLAN	06 MAY 2019
Drawn By: B. NOLAN	
Field By: WINTER-WILSON	
CAD File: GP 19-031 06 MAY 19.gxd	
Job #: 19-031	
Sheet #: 3 OF 3	

PROPOSED GRADING & DRAINAGE PLAN  
BC3 Ventures - Boyne City Site Plan Submittal

BENCH MARK  
TOP NUT on HYDRANT  
ELEV = 589.88 GPS DATUM



**PROPOSED SITE PLAN**  
BC3 Ventures, LLC Property  
Boyne City Site Plan Submittal 06 May 2019  
Part of SECTION 35, T33N, R6W  
BOYNE CITY, CHARLEVOIX COUNTY, MI



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06 MAY 2019	

**PROPOSED SITE PLAN**  
BC3 Ventures - Boyne City Site Plan Submittal



Integrated Architecture  
4050 Lake Drive SE  
Grand Rapids, MI 49546  
616.574.0920 P  
616.574.0953 F  
www.inlarch.com

Boyer City Lake Street

Boyer City, Michigan 49712

LANDSCAPE NOTES:

- CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND LOCAL PERMITS REQUIRED. ALL CONSTRUCTION SHALL CONFORM TO STATE AND LOCAL STANDARDS AND CODES THAT PERTAIN TO THE SITE UNDER CONSTRUCTION.
- CALL "MISS DIG", 1-800-452-7171, THREE (3) DAYS PRIOR TO THE START OF CONSTRUCTION, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS.
- EXISTING UNDERGROUND UTILITIES ARE PLOTTED FROM ORIGINAL SITE DRAWINGS AND SITE TOPOGRAPHIC SURVEY. EVERY ATTEMPT HAS BEEN MADE TO SHOW ALL UTILITY LINES WHERE THEY EXIST. CONTRACTOR SHALL USE EVERY PRECAUTION IN EXCAVATING FOR NEW UTILITIES SINCE ACTUAL UNDERGROUND UTILITIES MAY NOT BE AS SHOWN. CONTRACTOR SHALL MAKE HIMSELF THOROUGHLY FAMILIAR WITH ALL UNDERGROUND UTILITY LOCATIONS PRIOR TO ANY EXCAVATION AND VERIFY LOCATIONS AND DEPTHS OF ALL UTILITIES.
- CONTRACTOR SHALL PROVIDE HIS OWN LAYOUT, GRADING, STAKING, AND SURVEYING REQUIRED FOR CONSTRUCTION. REFER TO EXISTING TOPOGRAPHIC SURVEY FOR BENCHMARKS AND OTHER EXISTING INFORMATION. CONTRACTOR SHALL FIELD VERIFY LAYOUT PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL PROTECT ALL EXISTING AND NEW CONSTRUCTION FROM DAMAGE. SHOULD ANY DAMAGE OCCUR, CONTRACTOR SHALL MAKE ALL NECESSARY REPAIRS AT NO COST TO THE OWNER.
- LANDSCAPE CONTRACTOR SHALL COORDINATE HIS WORK WITH OTHER CONTRACTORS ON SITE TO MINIMIZE DAMAGE TO COMPLETED LAWN RESTORATION AND PLANT MATERIAL.
- IF ANY DISCREPANCY EXISTS BETWEEN THE QUANTITIES, SIZES OR MATERIALS INDICATED ON THE PLAN AND SHOWN IN THE PLANT LIST, THE PLAN SHALL GOVERN.
- IT IS THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE PRIOR TO BID SUBMITTAL, TO BECOME FAMILIAR WITH EXISTING CONDITIONS AT THE SITE.
- THE EARTHWORK CONTRACTOR IS RESPONSIBLE FOR ROUGH GRADING OF SUBGRADE BELOW LAWN AND LANDSCAPE AREAS.
- THE LANDSCAPE CONTRACTOR IS RESPONSIBLE FOR RESPREADING TOPSOIL, FINE GRADING AND PREPARATION OF ALL LAWN AND LANDSCAPE AREAS.
- PRIOR TO SPREADING TOPSOIL, THE LANDSCAPE CONTRACTOR SHALL INSPECT AND ACCEPT ALL BASE GRADES. ANY DEVIATION FROM LINE AND GRADE INDICATED ON THE GRADING PLAN SHALL BE CORRECTED BY EARTHWORK CONTRACTOR BEFORE PLACING ANY TOPSOIL.
- BEFORE PLANT MATERIAL INSTALLATION BEGINS, STAKE LOCATIONS OF NEW PLANT MATERIAL AND NOTIFY ARCHITECT FOR APPROVAL. NOTIFY ARCHITECT ONE WEEK MINIMUM PRIOR TO TENTATIVE DATE OF STAKING.
- CONTRACTOR SHALL NOTIFY ARCHITECT IF AREAS OF POOR DRAINAGE OR OTHER UNUSUAL SUBSURFACE CONDITIONS ARE ENCOUNTERED DURING EXCAVATION FOR PLANTING PITS.
- ALL GRADING, UTILITY, AND IRRIGATION WORK SHALL BE COMPLETED PRIOR TO INSTALLATION OF PLANT MATERIAL AND LANDSCAPE MULCH.
- ALL SHRUB BEDS ADJACENT TO LAWN AREAS SHALL HAVE A METAL EDGE BORDER.
- ALL SHRUB BED AREAS SHALL HAVE A MINIMUM 6-INCH THICK LAYER OF LANDSCAPE MULCH AT TIME OF PLANTING.
- PRE-EMERGENT HERBICIDE SHALL BE USED UNDER MULCH IN ALL LARGE BED AREAS.
- LANDSCAPE CONTRACTOR SHALL SEED ALL AREAS DISTURBED BY CONSTRUCTION NOT DESIGNATED TO BE SODED.
- LANDSCAPE CONTRACTOR SHALL WORK OVER LAWN AREAS THAT HAVE REMAINED PARTIALLY INTACT. TOP DRESSING WITH SOIL, SCARIFYING, AND SEEDING TO FORM A SMOOTH, FULL, EVEN LAWN, FREE OF BARE SPOTS, INDENTATIONS, AND WEEDS.
- LANDSCAPE CONTRACTOR SHALL COORDINATE PLANTING SCHEDULE WITH IRRIGATION CONTRACTOR TO ENSURE PROPER WATERING OF PLANTED AND SEEDED AREAS AFTER INSTALLATION. LANDSCAPE CONTRACTOR SHALL COORDINATE PLANTING WITH IRRIGATION CONTRACTOR TO MINIMIZE CONFLICTS WITH SPRINKLER HEADS AND LINES.
- TOPSOIL: TOPSOIL SHALL BE INSTALLED WITH A MINIMUM DEPTH OF FOUR (4) INCHES FOR LAWN AREAS AND TWELVE (12) TO FIFTEEN (15) INCHES WITHIN PLANTING BEDS.

**SITE CONTRACTOR NOTES:**

PROVIDE 8" THICK WOOD MULCH IN ALL SHRUB PLANTING BEDS.

PROVIDE 12" PLANTING SOIL IN ALL PLANTING BED AREAS

MULCH RING AROUND TREES TO HAVE A SPADED EDGE AND AT A MINIMUM BE 8" IN DIAMETER.

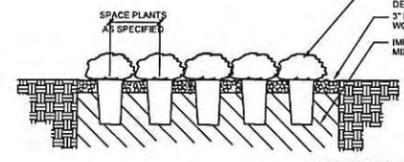
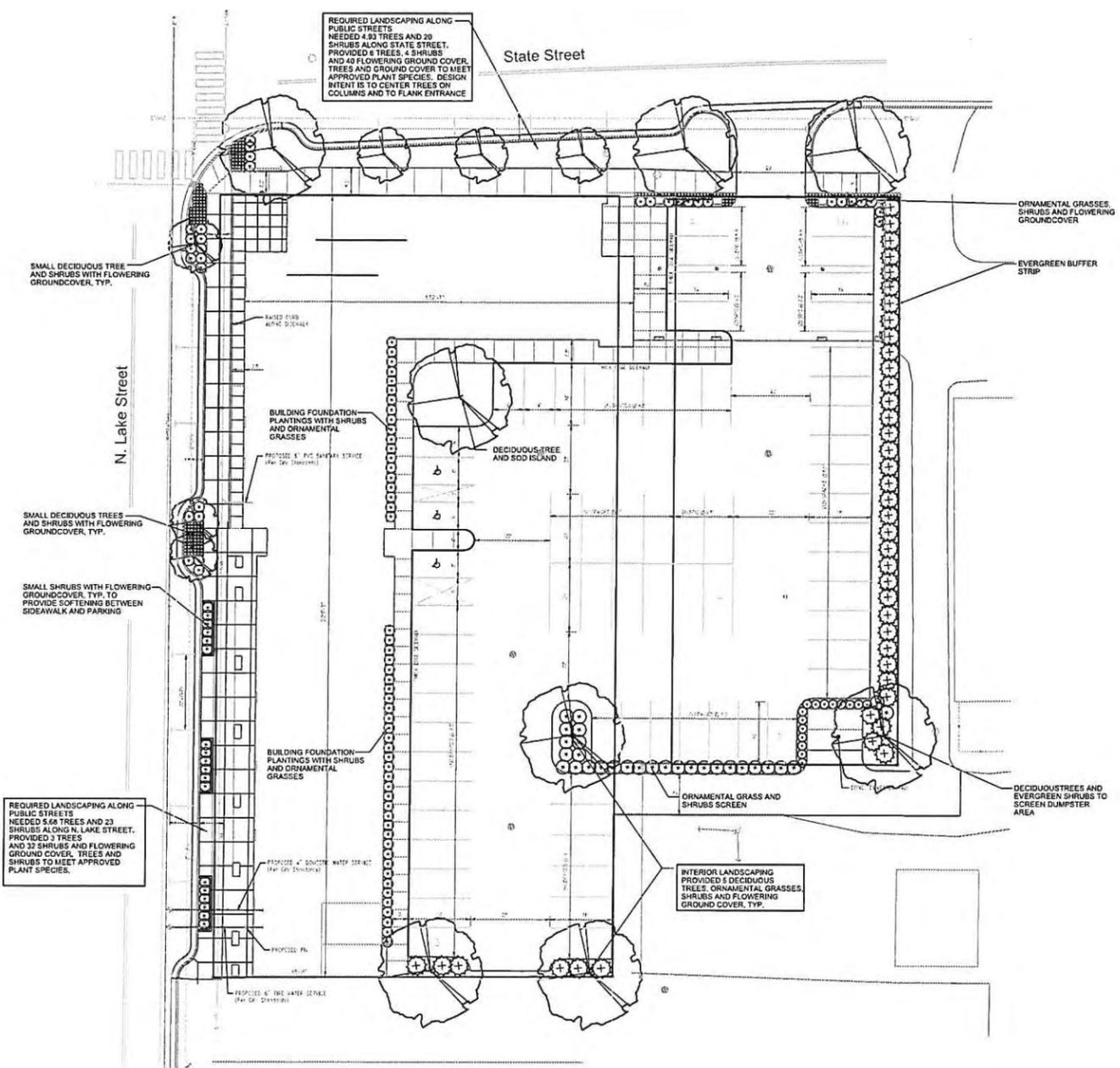
ALL PROPOSED LANDSCAPE AREAS; ALONG STREETS WITHIN PARKING AREAS, NEXT TO BUILDING, ETC. ARE TO BE IRRIGATED

ALL ISLANDS WITHIN PARKING AREA TO BE CURBED.

MINIMUM WIDTH OF PARKING AREA ISLANDS SHALL BE 10 FEET WIDE AT ENDS FACING MAIN AISLES.

ALL PLANT MATERIAL TO BE CHOSEN FROM CITY'S APPROVED LIST OF PLANT MATERIAL AND MEET CITY REQUIREMENTS. 2 1/2" CAL. FOR DECIDUOUS AND ORNAMENTAL TREES 8" TALL FOR EVERGREEN TREES SHRUBS TO BE 24" IN HEIGHT MINIMUM

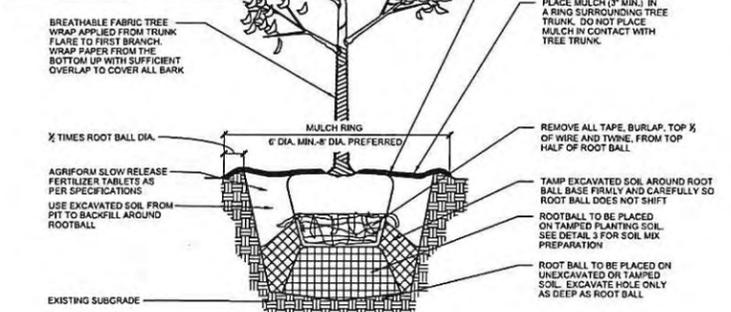
STEEL EDGING TO BE INSTALLED WHERE LAWN MEETS PLANTING BEDS



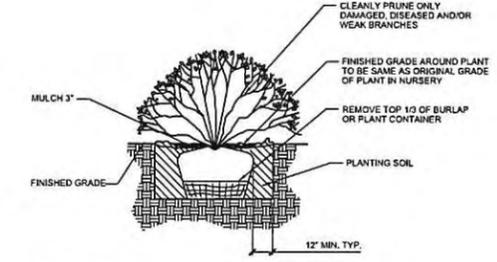
**1 PERENNIAL / ANNUAL PLANTING DETAIL**  
LP1.0 NOT TO SCALE:

TOPSOIL PREPARATION FOR PERENNIALS/ANNUALS  
PLACE 4" LAYER OF FINE WOODCHIP MULCH AND 6 LBS OF 5-10-5 FERTILIZER PER 100 SF OVER TOPSOIL IN BED AREA. ROTOTILL TO A MINIMUM DEPTH OF 12". TEST TOPSOIL MIXTURE FOR PH LEVEL. ADJUST PH TO REGISTER BETWEEN 6 AND 6.5. PREPARE PLANTING BED MINIMUM TWO WEEKS PRIOR TO PLANTING

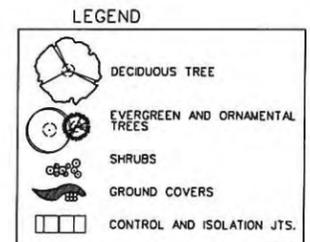
NOTE: INSTALL TREE WRAP AT TIME OF PLANTING. AS DIRECTED BY LANDSCAPE ARCHITECT, REMOVE NO LATER THAN 1 YEAR AFTER PLANTING. WRAP TREES WITH VERY THIN BARK. (RED MARKS) AS DIRECTED BY LANDSCAPE ARCHITECT. USE WOODEN STAKES WITH NYLON TIES WHEN STAKING TREES



**2 TREE PLANTING DETAIL**  
LP1.0 NOT TO SCALE:



**3 SHRUB PLANTING DETAIL**  
LP1.0 NOT TO SCALE:



Boyer City Site Plan Submittal 06 May 2019

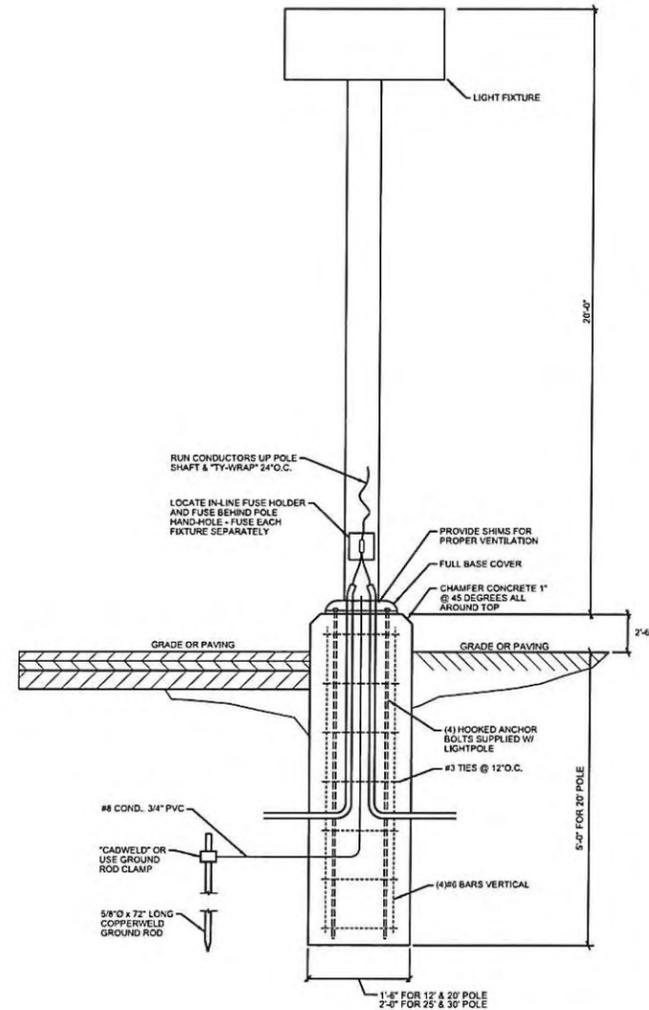
Boyer City Pre-app 13 Feb. 2019

Design	KAB
PM / PA	DD / AT
Drawn	KAB
IA Project Number	20181117

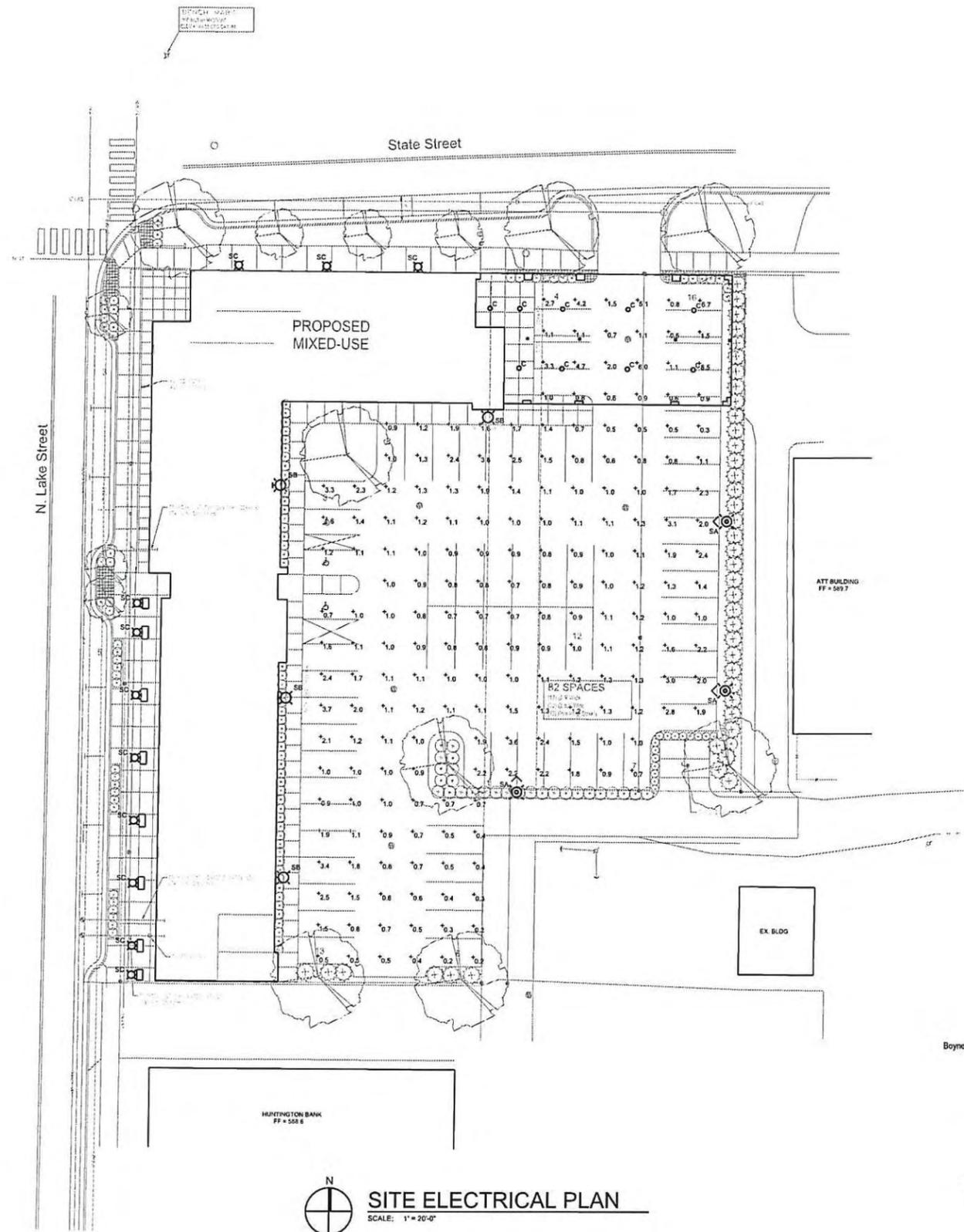
**SITE LANDSCAPE PLAN**

**LP1.0**

**SITE LANDSCAPE PLAN**  
SCALE: 1" = 20'-0"



1 OUTDOOR LIGHTING STANDARD RAISED CONCRETE BASE DETAIL  
 SCALE: NTS



SITE ELECTRICAL PLAN  
 SCALE: 1" = 20'-0"

Boyer City Site Plan Submittal	06 May 2019
Boyer City Pre-app	13 Feb. 2019
Design	WS
PM / PA	DD / AT
Drawn	WS
IA Project Number	20181117

SITE ELECTRICAL PLAN

# RA35-12F

# LUNA™ LED

## 3.5" LUNA™ LED Round Fixed Recessed Fixture Recess Lighting

Job Information		
Project Name		Type
Location		C
Quantity		Date
Contact/Phone		
Notes		

### Features

#### Body

Durable airtight fixed aluminum body. Includes anodized blue heat sink to provide maximum airflow for the LED light source.

#### LED Characteristics

This general application light fixture features a 12 watt LED module that maintains uniform intensity producing up to 885 lumens; with a typical CRI of 90. Available in 2700 K, 3000 K, 3500 K, 4000 K or 5000 K colour temperature.

#### Dimming

100%-10% dimming capability. This fixture is compatible with industry standard TRIAC/ELV dimmers. For compatible dimmers, visit [www.liteline.com/lunadimmers](http://www.liteline.com/lunadimmers)

#### Beam Spread

The fixture lens provides 40° beam spread.

#### Mounting

Includes two spring loaded clips to attached to any ceiling material.

#### LED Driver

Extruded aluminum hardwire box with 300mA dimmable class II electronic LED driver with 100-135V AC input. Includes an 18" FT6 rated cable with DC 2.1 connector between driver and fixture approved for use in plenums and suspended ceilings.

#### Quick Connect push-in terminals

Three "Quick connect" push-in terminals for fast and easy wiring.

#### Operating Temperature

-20°C~40°C (-4°F~104°F)

#### Environment

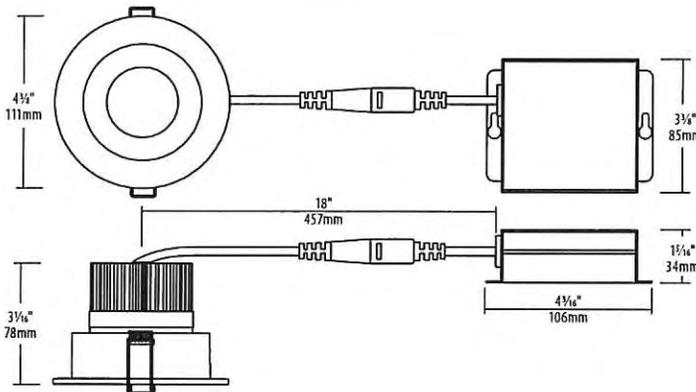
- Suitable for wet locations.
- Approved for direct contact with insulation.

SPECIFICATION	
Application	Ceiling Recess Mount
Approved Location	Wet / Insulated Ceilings
Beam Angle	40°
CCT	2700 K / 3000 K / 3500 K / 4000 K / 5000 K
Certification	cETLus
Class II	Yes
Colour	BK / BN / SL / WH
CRI	90
Dimming	Yes
Dimming Tech	TRIAC/ELV
Energy Star	Yes
Hole Cut	Ø 3 1/4" (92mm)
Input	100-135V AC, 60Hz
LM79	Yes
LM80	Yes
Lumens	800 / 830 / 850 / 865 / 885
Lumens per Watt	69 / 71 / 72 / 73 / 75
Power Factor	>0.90
Projected Life	70% @ 50,000 hrs
Warranty	Lifetime
Wattage	12W



### Description

The RA35-12F is a 12 watt 3.5" round fixed recessed light fixture for retrofit application. It is a high-performance, easy to install downlight solution that offers good light output, energy-efficiency and streamlined design. Suitable for wet locations.



**Notes:** Accessories are sold separately. For additional options please consult your Liteline representative. Due to our continued efforts to improve our products, product specifications are subject to change without notice.

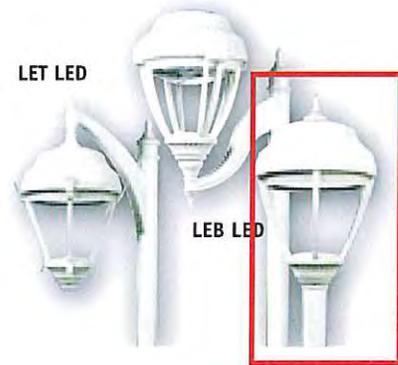
### Ordering Guide

RA35-12F -	Colour Temperature	Driver Options	90	Trim Finish
	<p>27K : 2700 K</p> <p>30K : 3000 K</p> <p>35K : 3500 K</p> <p>40K : 4000 K</p> <p>50K : 5000 K</p>	<p>Leave blank for standard 120V driver</p> <p>L3 : Lutron Hi-Lume® 1% (3 Wire)</p> <p>E : Lutron Hi-Lume® 1% (EcoSystem™)</p> <p>EM : Emergency Backup, 120-277V, 0-10V</p> <p>C : 120-347V, 0-10V</p>	<p>BK : Black</p> <p>BN : Brushed Nickel</p> <p>SL : Silver</p> <p>WH : White</p>	

### SA

#### FEATURES

- LED PicoEmitter technology with up to 25% boost in lumen output
- Heritage style meets modern full-cutoff performance
- Three arm options; Single Straight, Single Curved, and Double Curved
- Three decorative accent options; medallion with DC Arm, leaflets with SS Arm, and top housing finial



3000K and warmer CCTs only

#### ORDERING INFORMATION (Example)

<b>1A</b>	<b>LET</b>	<b>19</b>	<b>SS</b>	<b>3E35</b>	<b>60L5K120</b>	<b>WH</b>	<b>LEAF/A-30</b>	<b>HSAS14-534188SA-HA11S/WH</b>
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MOUNTING		EPA 19'
• FM	Flush Mt.	1.0
• PT	Pipe Tenon Mt.	1.0
• 1A	1 Arm Side Mt.	0.7
• 2B	2 Arm Side Mt.	1.4
• 3Y	3 Arm Side Mt.	2.1
• 4C	4 Arm Side Mt.	2.4
• 1W	Single Wall Mt.	n/a

NOTE: EPA is for Fixture only

SUPPORT	
• S7F	19" Dia.
• LEB	Bottom Mt.
• LET	Top Mt.

FIXTURE ARM	
• SS	Single Straight Arm
• SC	Single Curved Arm
• DC	Double Curved Arm

ELECTRICAL MODULE <sup>3</sup>		
Source	Color Temperature	Voltages
60L 60 LEDs, 67W	2K 580nm	120 120V
	3K 3000K	208 208V
	4K 4200K	240 240V
	5K 5100K	277 277V
		347 347V <sup>1</sup>
		480 480V <sup>1</sup>

FIXTURE FINISH	
• BL	Black
• DB	Dark Bronze
• LG	Light Gray
• SG	Stealth Gray
• PS	Platinum Silver
• WH	White
• CC	Custom Color*

\*Consult representative

POLE / POLE ARM	
See Arms & Poles Selection Guide at <a href="http://www.kimlighting.com">www.kimlighting.com</a> for pole EPA & ordering.	
1W Wall Mount arm not included and must be ordered separately.	

FIXTURE OPTIONS		
• LEAF	Leaflets, top & bottom of arm (SS only)	• SF 120 Volt Single Fuse
• MDL	Medallions, top of arm (DC only)	• DF 208 Volt Double Fuse
• TF	Finial (bottom mt. fixture only)	• DF 240 Volt Double Fuse
• A-30	120 Volt photocell	• SF 277 Volt Single Fuse
• A-31	208 Volt photocell	• SF 347 Volt Single Fuse
• A-32	240 Volt photocell	• DF 480 Volt Double Fuse
• A-33	277 Volt photocell	• SCL-R Pole Occupancy Sensor up to 16' ht., Round <sup>2</sup>
• A-34	480 Volt photocell	• SCL-S Pole Occupancy Sensor up to 16' ht., Square <sup>2</sup>
• A-35	347 Volt Photocell	• SCH-R Pole Occupancy Sensor up to 30' ht., Round <sup>2</sup>
• NFO	Neighbor Friendly Optic	• SCH-S Pole Occupancy Sensor up to 30' ht., Square <sup>2</sup>

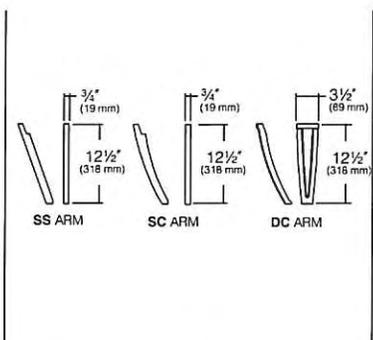
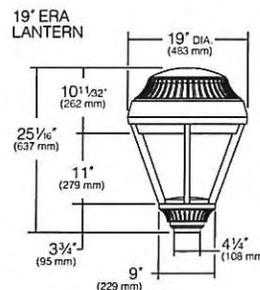
FIXTURE	
Distribution	Current
1 Type I Full Cutoff	E35 350mA PicoEmitter
2 Type II Full Cutoff	
3 Type III Full Cutoff	
4 Type IV Full Cutoff	
5 Type V Full Cutoff	
R Type R Right Full Cutoff	
L Type L Left Full Cutoff	

<sup>1</sup> Due to current unavailability of 347V and 480V drivers, specification of these voltages may feature an integral step-down transformer.

<sup>2</sup> Specify pole diameter if round, voltage and color, e.g. SCL-R4/277/BL.

<sup>3</sup> Driver has a 0-10V dimming interface with a dimming range of 10-100%. Compatible with most control systems.

HID to LED Upgrade Kits available. Contact factory.

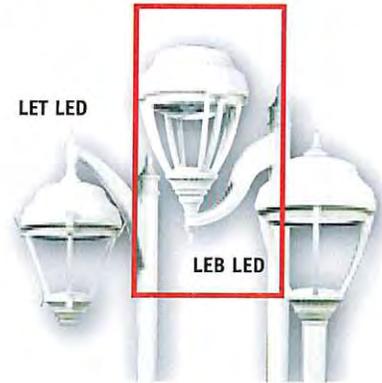


## SB

## Era® Lantern with PicoEmitter®

### FEATURES

- LED PicoEmitter technology with up to 25% boost in lumen output
- Heritage style meets modern full-cutoff performance
- Three arm options; Single Straight, Single Curved, and Double Curved
- Three decorative accent options; medallion with DC Arm, leaflets with SS Arm, and top housing finial



3000K and warmer CCTs only

### ORDERING INFORMATION (Example)

<b>1A</b>	<b>LET</b>	<b>19</b>	<b>SS</b>	<b>3E35</b>	<b>60L5K120</b>	<b>WH</b>	<b>LEAF/A-30</b>	<b>HSAS14-534188SA-HA11S/WH</b>
<b>MOUNTING</b>		<b>SIZE</b>		<b>FIXTURE FINISH</b>		<b>POLE / POLE ARM</b>		
<ul style="list-style-type: none"> <li>• <b>FM</b> Flush Mt. 1.0</li> <li>• <b>PT</b> Pipe Tenon Mt. 1.0</li> <li>• <b>1A</b> 1 Arm Side Mt. 0.7</li> <li>• <b>2B</b> 2 Arm Side Mt. 1.4</li> <li>• <b>3Y</b> 3 Arm Side Mt. 2.1</li> <li>• <b>4C</b> 4 Arm Side Mt. 2.4</li> <li>• <b>1W</b> Single Wall Mt. n/a</li> </ul>		<ul style="list-style-type: none"> <li>• <b>19</b> 19" Dia.</li> </ul>		<ul style="list-style-type: none"> <li><b>BL</b> Black</li> <li><b>DB</b> Dark Bronze</li> <li><b>LG</b> Light Gray</li> <li><b>SG</b> Stealth Gray</li> <li><b>PS</b> Platinum Silver</li> <li><b>WH</b> White</li> <li><b>CC</b> Custom Color*</li> </ul>		<p>See Arms &amp; Poles Selection Guide at <a href="http://www.kimlighting.com">www.kimlighting.com</a> for pole EPA &amp; ordering.</p> <p>1W Wall Mount arm not included and must be ordered separately.</p>		
<b>NOTE: EPA is for Fixture only</b>		<b>SUPPORT</b>		<b>ELECTRICAL MODULE<sup>3</sup></b>		<b>FIXTURE OPTIONS</b>		
		<ul style="list-style-type: none"> <li>• <b>LEB</b> Bottom Mt.</li> <li>• <b>LET</b> Top Mt.</li> </ul>		<ul style="list-style-type: none"> <li><b>Source</b></li> <li><b>60L</b> 60 LEDs, 67W</li> </ul>		<ul style="list-style-type: none"> <li><b>LEAF</b> Leaflets, top &amp; bottom of arm (SS only)</li> <li><b>MDL</b> Medallions, top of arm (DC only)</li> <li><b>TF</b> Finial (bottom mt. fixture only)</li> <li><b>A-30</b> 120 Volt photocell</li> <li><b>A-31</b> 208 Volt photocell</li> <li><b>A-32</b> 240 Volt photocell</li> <li><b>A-33</b> 277 Volt photocell</li> <li><b>A-34</b> 480 Volt photocell</li> <li><b>A-35</b> 347 Volt Photocell</li> <li><b>NFO</b> Neighbor Friendly Optic</li> </ul>		
<b>FIXTURE</b>		<b>Current</b>		<b>Color Temperature</b>		<b>Volts</b>		
<ul style="list-style-type: none"> <li><b>Distribution</b></li> <li>1 Type I Full Cutoff</li> <li>2 Type II Full Cutoff</li> <li>3 Type III Full Cutoff</li> <li><b>4 Type IV Full Cutoff</b></li> <li>5 Type V Full Cutoff</li> <li>R Type R Right Full Cutoff</li> <li>L Type L Left Full Cutoff</li> </ul>		<ul style="list-style-type: none"> <li><b>E35</b> 350mA PicoEmitter</li> </ul>		<ul style="list-style-type: none"> <li><b>2K</b> 580nm</li> <li><b>3K</b> 3000K</li> <li><b>4K</b> 4200K</li> <li><b>5K</b> 5100K</li> </ul>		<ul style="list-style-type: none"> <li><b>SF</b> 120 Volt Single Fuse</li> <li><b>DF</b> 208 Volt Double Fuse</li> <li><b>DF</b> 240 Volt Double Fuse</li> <li><b>SF</b> 277 Volt Single Fuse</li> <li><b>SF</b> 347 Volt Single Fuse</li> <li><b>DF</b> 480 Volt Double Fuse</li> <li><b>SCL-R</b> Pole Occupancy Sensor up to 16' ht., Round<sup>2</sup></li> <li><b>SCL-S</b> Pole Occupancy Sensor up to 16' ht., Square<sup>2</sup></li> <li><b>SCH-R</b> Pole Occupancy Sensor up to 30' ht., Round<sup>2</sup></li> <li><b>SCH-S</b> Pole Occupancy Sensor up to 30' ht., Square<sup>2</sup></li> </ul>		
				<ul style="list-style-type: none"> <li><b>Volts</b></li> <li>120 120V</li> <li>208 208V</li> <li>240 240V</li> <li>277 277V</li> <li>347 347V<sup>1</sup></li> <li>480 480V<sup>1</sup></li> </ul>				

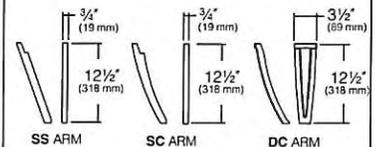
<sup>1</sup> Due to current unavailability of 347V and 480V drivers, specification of these voltages may feature an integral step-down transformer.

<sup>2</sup> Specify pole diameter if round, voltage and color, e.g. SCL-R4/277/BL.

<sup>3</sup> Driver has a 0-10V dimming interface with a dimming range of 10-100%. Compatible with most control systems.

HID to LED Upgrade Kits available. Contact factory.

19" ERA LANTERN



## VARELLO 220 LED

### Specification

The simple cylindrical form of VARELLO L blends with a broad range of architecture. The heavy wall extruded aluminum housing, fitted top and bottom with precision-machined lens rings, provides subtle direct and indirect illumination. Luminaire is available in two sizes and is finished in finely textured paint. Hardware is stainless steel.



---

Model	Lamp	Color Temperature	Volt	Mounting	Beam	Finish	Option
VL220 LED	LED	WW - 3000K	UNV - 120-277V	W - Wall Mount	NB - 13°	SG - Silver Grey	DIM - 0-10VDC Dimming
		NW - 4000K			MB - 27°	DG - Dark Grey	N - None
					WB - 59°	GG - Graphite Grey	
						BLK - Matte Black	
						BRZ - Dark Bronze	
						CC - Custom Color	

### Ordering Information

Specifications are subject to change without notification

## VARELLO 220 LED

Specification

### HOUSING

Cylindrical housing is machined from 6060 extruded aluminum alloy tubing. Upper and lower lens rings are precision machined from 6060 aluminum and sealed to the housing with one-piece die-cut closed cell silicone gaskets. Upper and lower tempered glass lenses are sealed to the housing with one-piece cell silicone gaskets. Luminaire with cast aluminum bracket mounts to wall surface with factory-supplied wall anchors and hardware. All hardware is stainless steel.

### OPTICS

Upper and lower optics consist of single CoB LED with reflector and clear tempered flat glass lens. Available distributions include narrow 13°, medium 27°, or wide 59° beam spreads.

### ELECTRICAL

Two integral high-power factor electronic LED drivers supply 350mA drive current to the LED modules at standard output or 700mA for high output models. Input voltage range is 120v to 277v at 50/60Hz. Total power consumption is 25watts for standard output and 50 watts for high output.

DELIVERED LUMENS (Top and bottom LED module combined) / CRI = 80

3000K - Standard output / High output

13° beam - 2914 lumens / 5828 lumens

27° beam - 2796 lumens / 5592 lumens

59° beam - 2720 lumens / 5440 lumens

4000K - Standard output / High output

13° beam - 3108 lumens / 6216 lumens

27° beam - 2985 lumens / 5970 lumens

59° beam - 2902 lumens / 5804 lumens

NOTE: Due to rapid and continuous advances in LED technology, LED luminaire data is subject to change without notice and at the discretion of HessAmerica. Consult factory for current technical data.

### FINISH

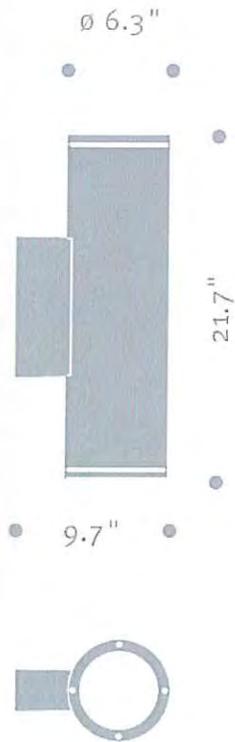
Housing is cleaned ultrasonically prior to painting. Standard finish is finely textured matte silver grey metallic, dark grey, graphite grey, matte black, or dark bronze. Special colors available on request.

### WARRANTY

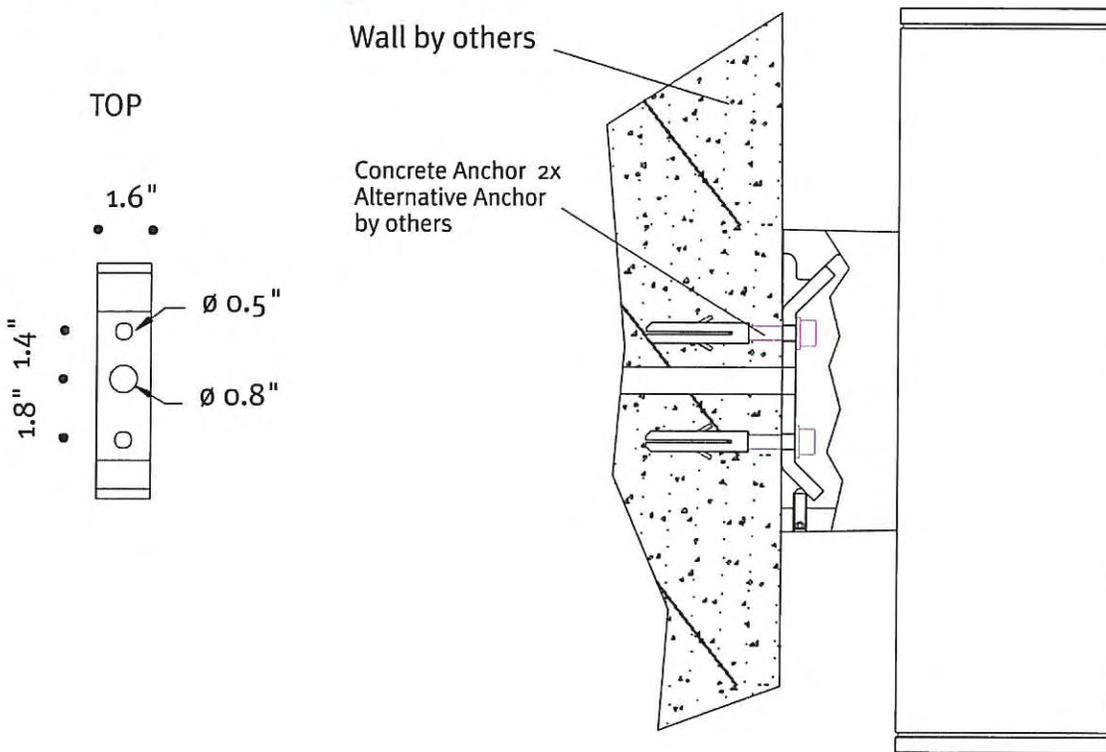
Limited product warranty period including LEDs is five years. Driver shall carry the manufacturer's limited warranty.

## Additional information

### Dimensions



### Mounting detail



Specifications are subject to change without notification



**BOYNE CITY**  
LAKE STREET DEVELOPMENT

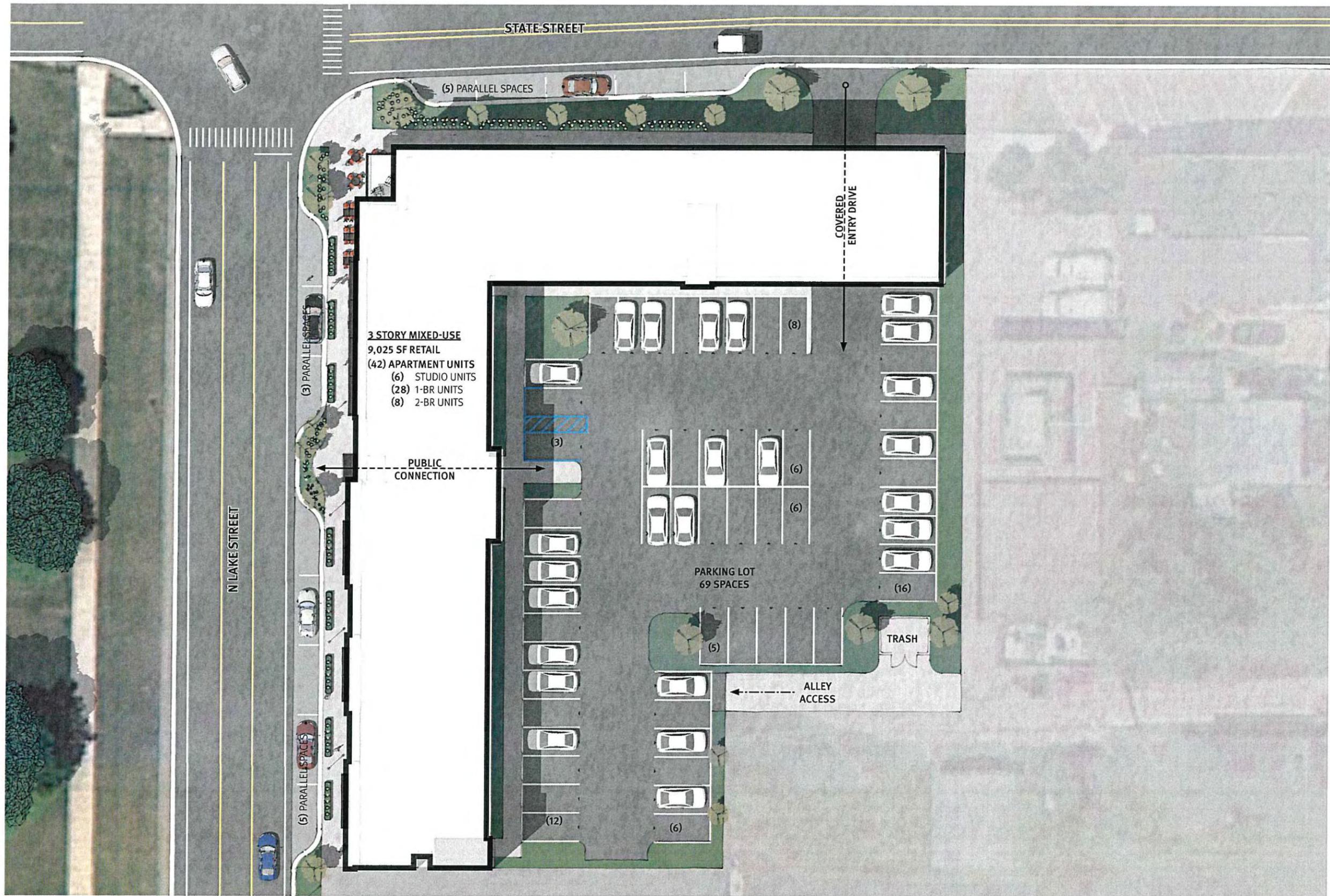
SITE PLAN SUBMITTAL  
**06 MAY 2019**

**T A B L E . O F . C O N T E N T S**

EXISTING SITE AERIAL_____	1
PROPOSED SITE PLAN_____	2
EXTERIOR IMAGES_____	3-4
PROPOSED EXTERIOR ELEVATIONS_____	5
ARCHIVE_____	6-16





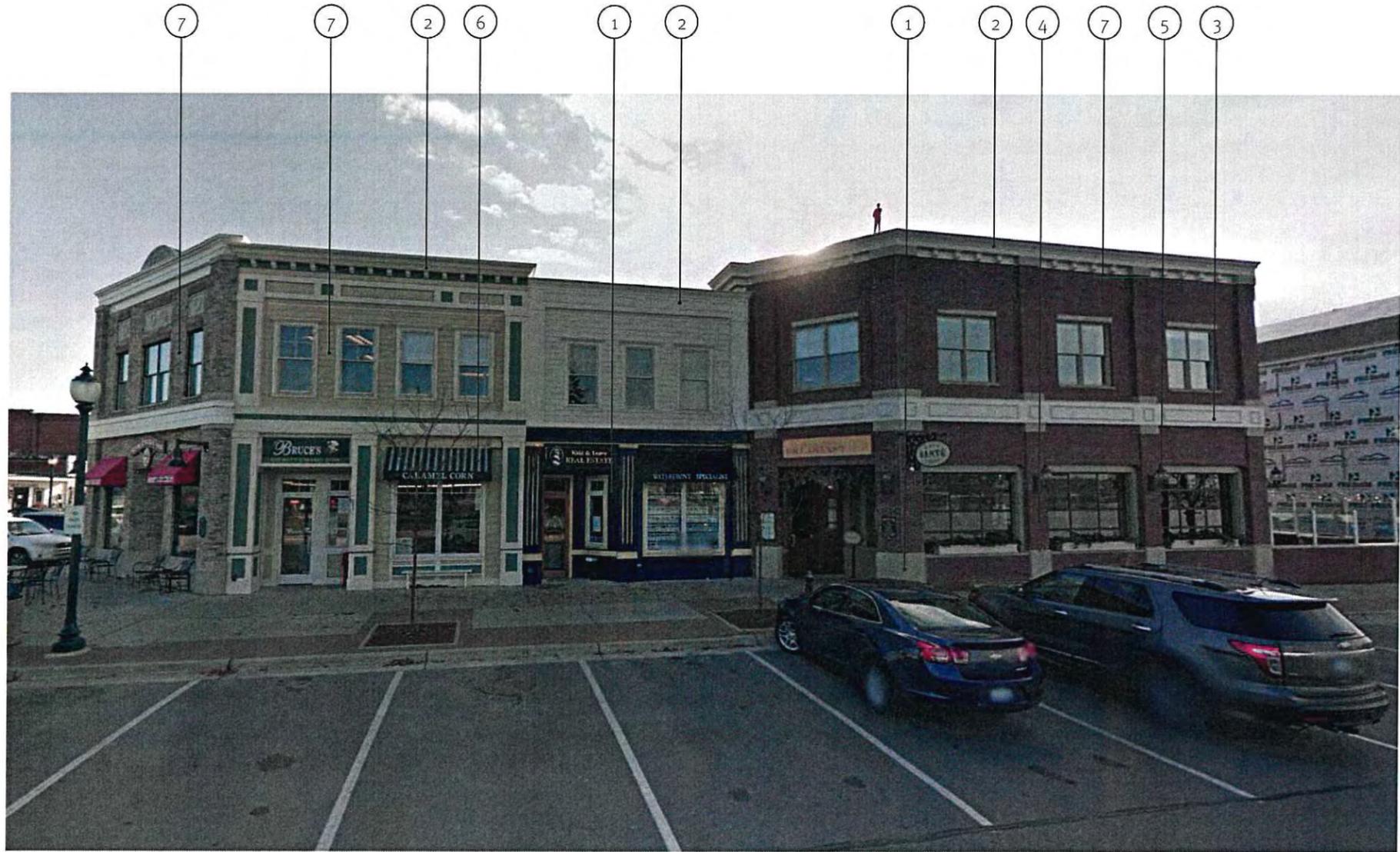








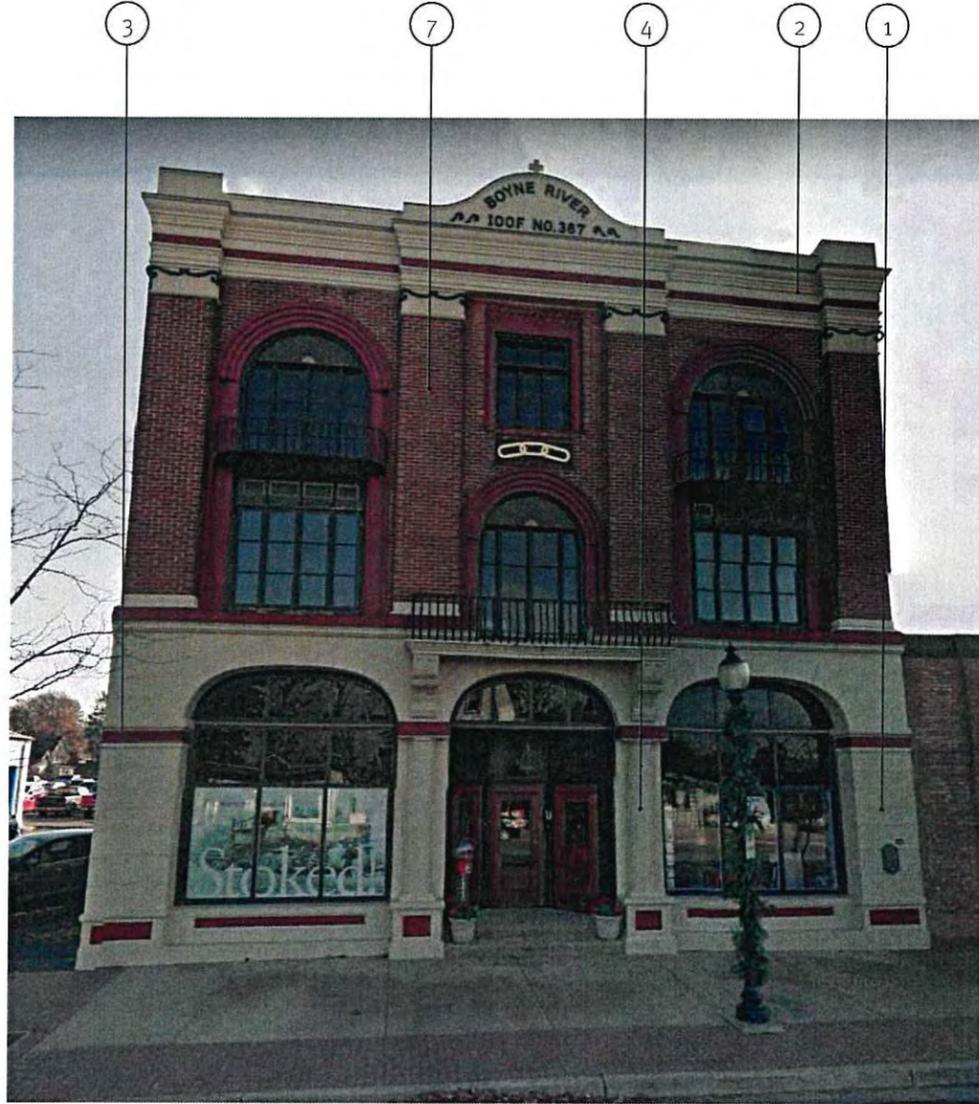
A R C H I V E



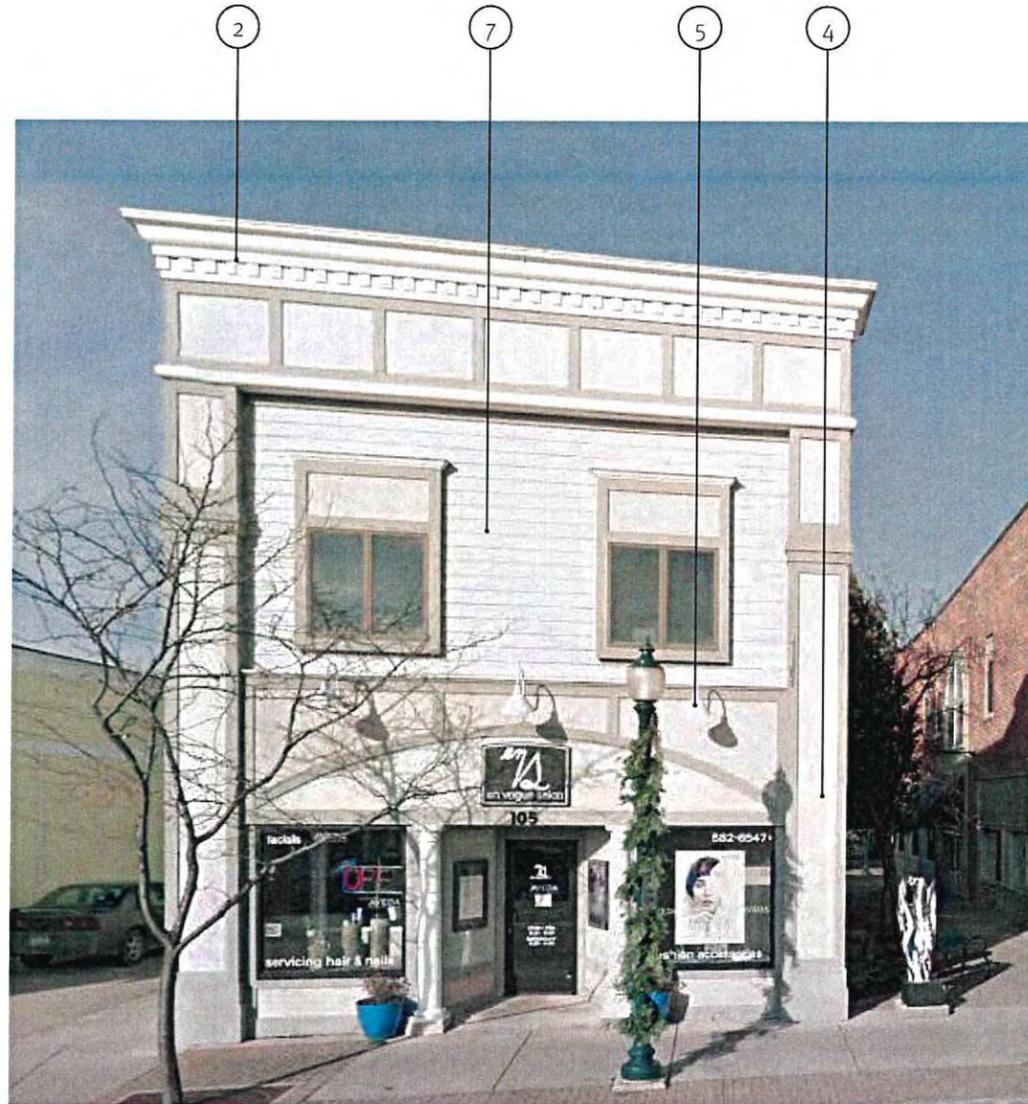
WATERS STREET

**BOYNE CITY EXISTING ARCHITECTURE  
DEVELOPMENT REQUIREMENTS**

- ① Contrasting base
- ② Roof cornice
- ③ Contrasting masonry courses
- ④ Columns / Collonade
- ⑤ Decorative light fixtures
- ⑥ Door / Window canopies
- ⑦ Brick / Stone / Wood facade material



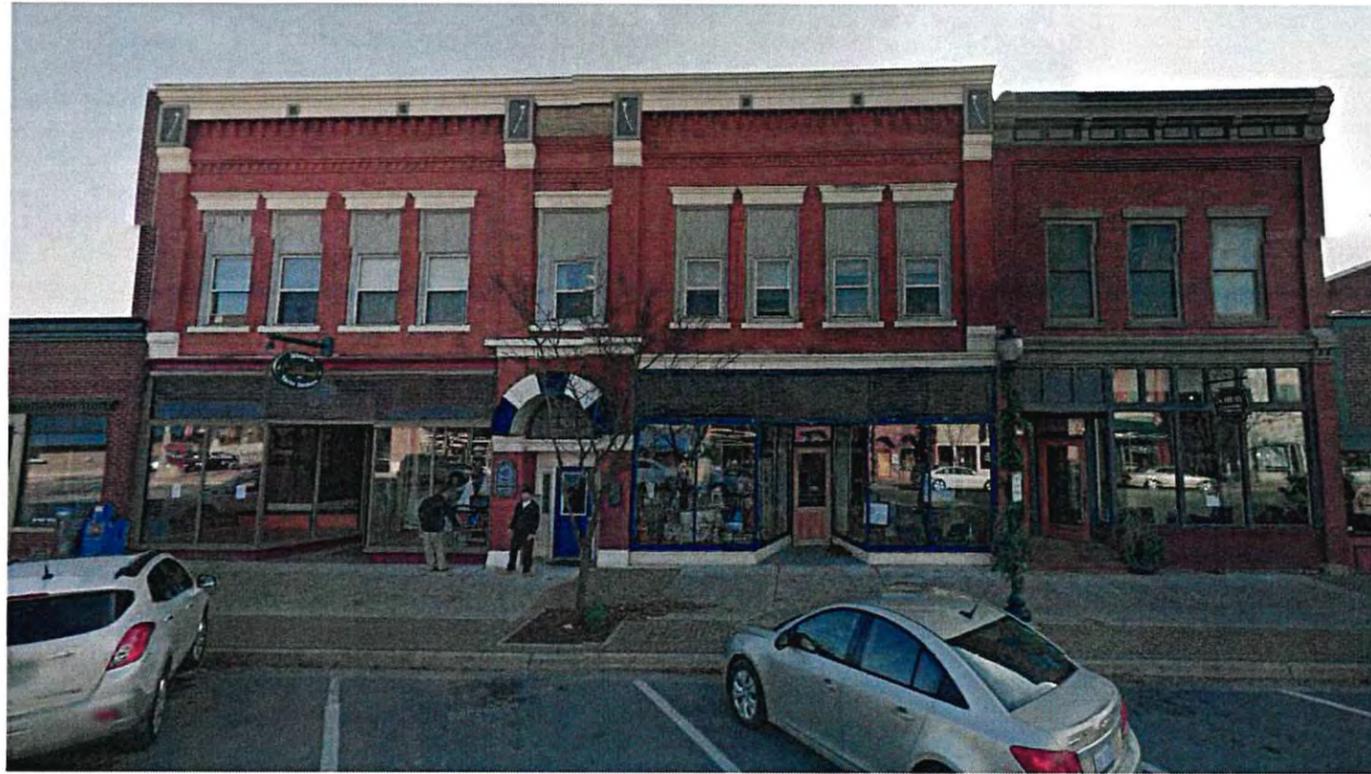
WATERS STREET



WATERS STREET

**BOYNE CITY EXISTING ARCHITECTURE  
DEVELOPMENT REQUIREMENTS**

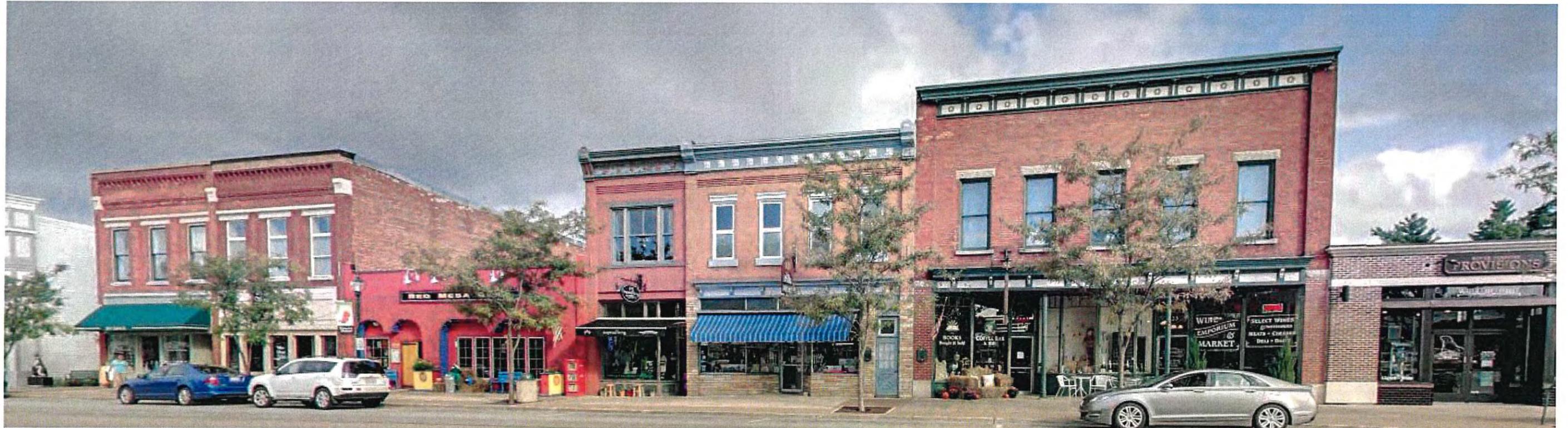
- ① Contrasting base
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WATERS STREET



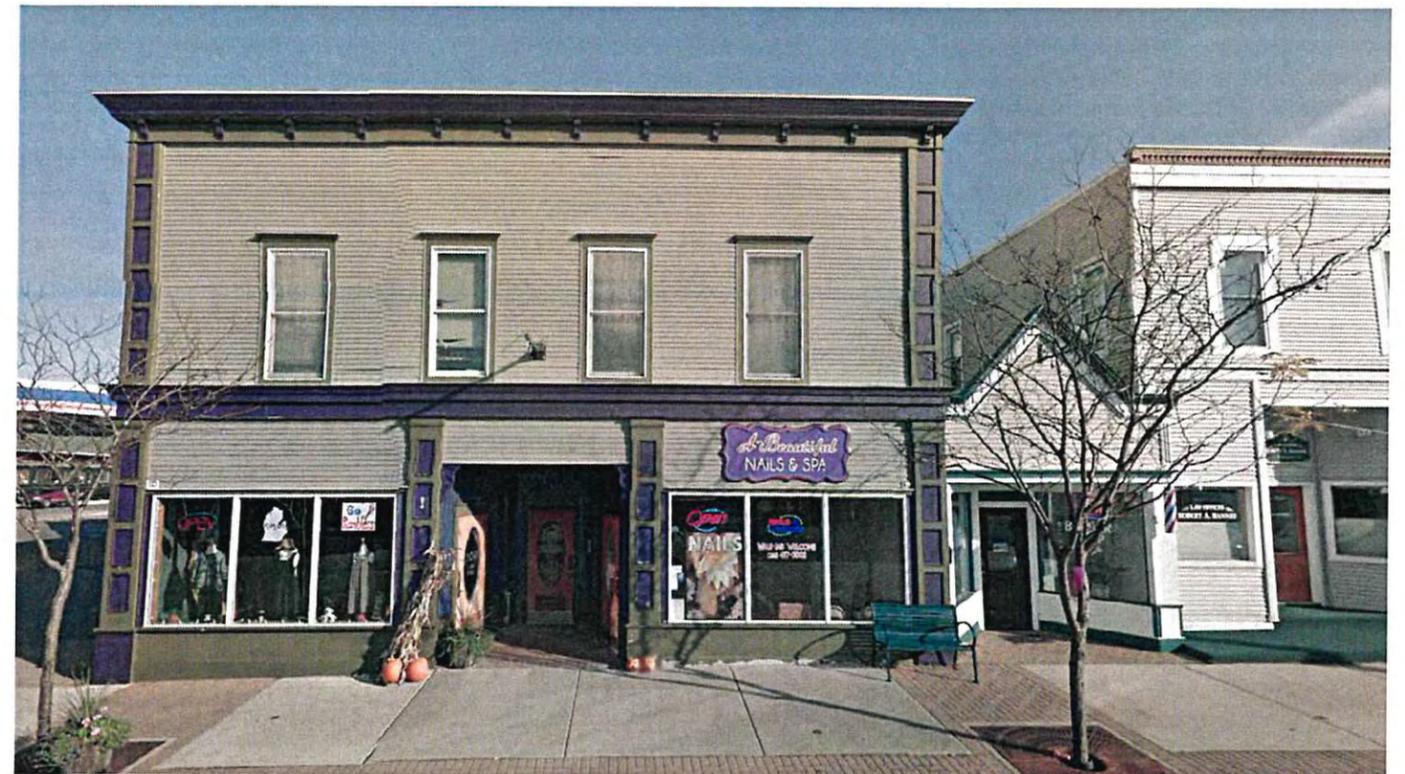
BOYNE AVENUE



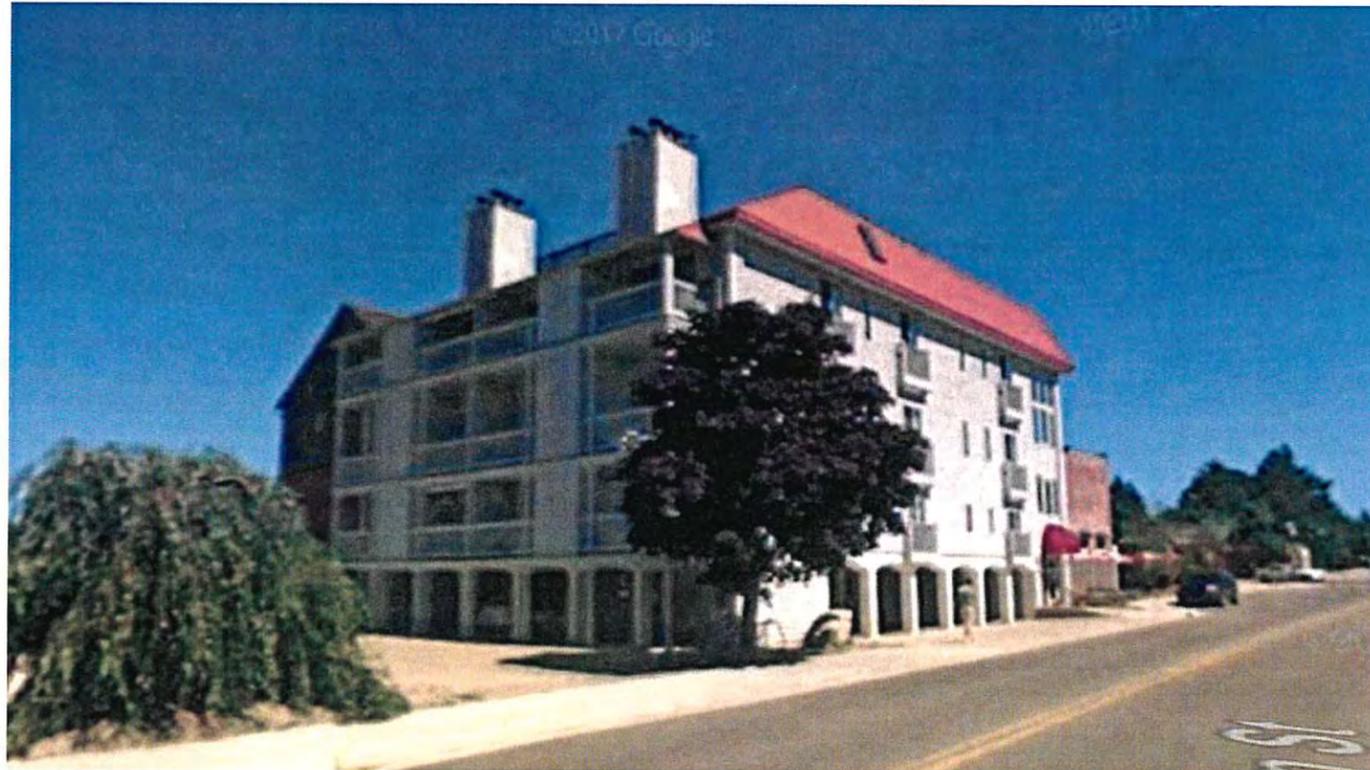
WATERS STREET



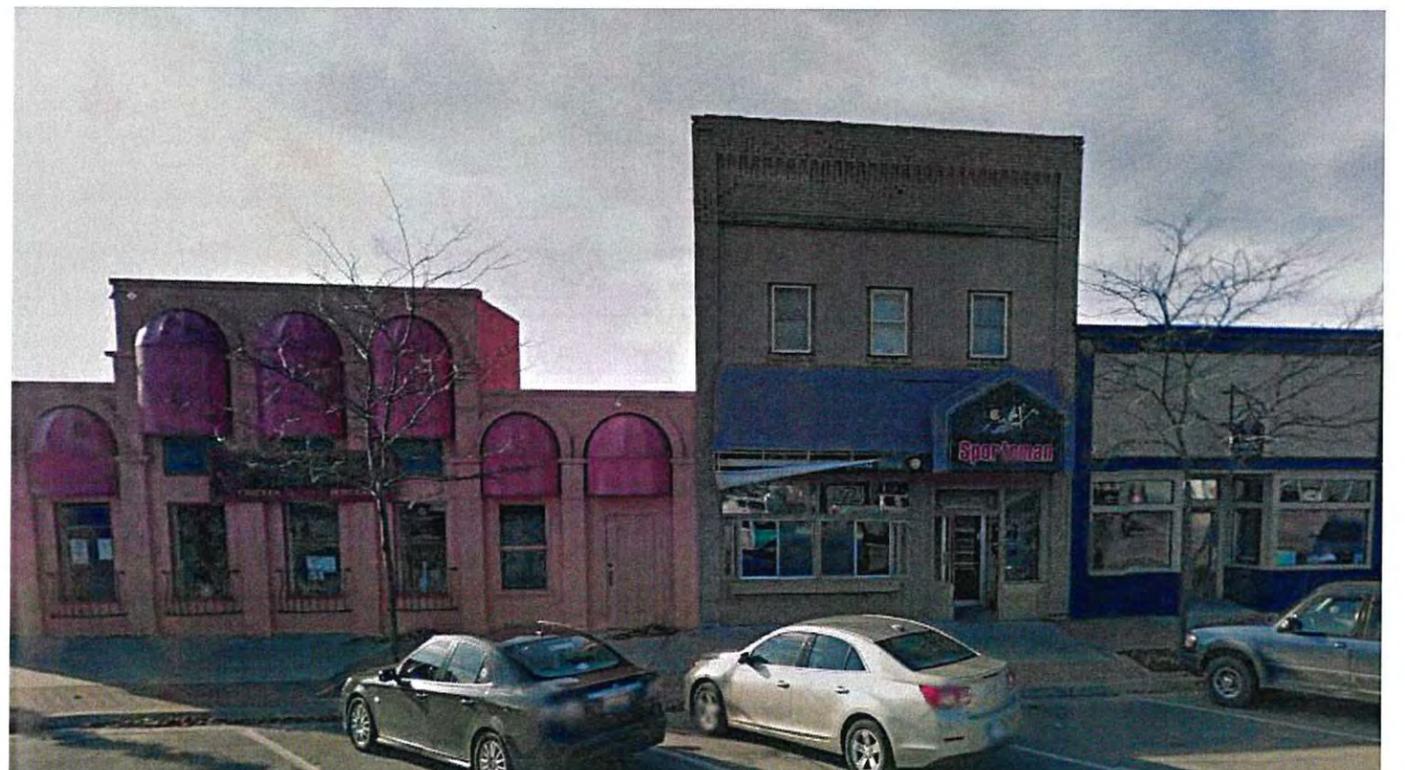
LAKE STREET



LAKE STREET



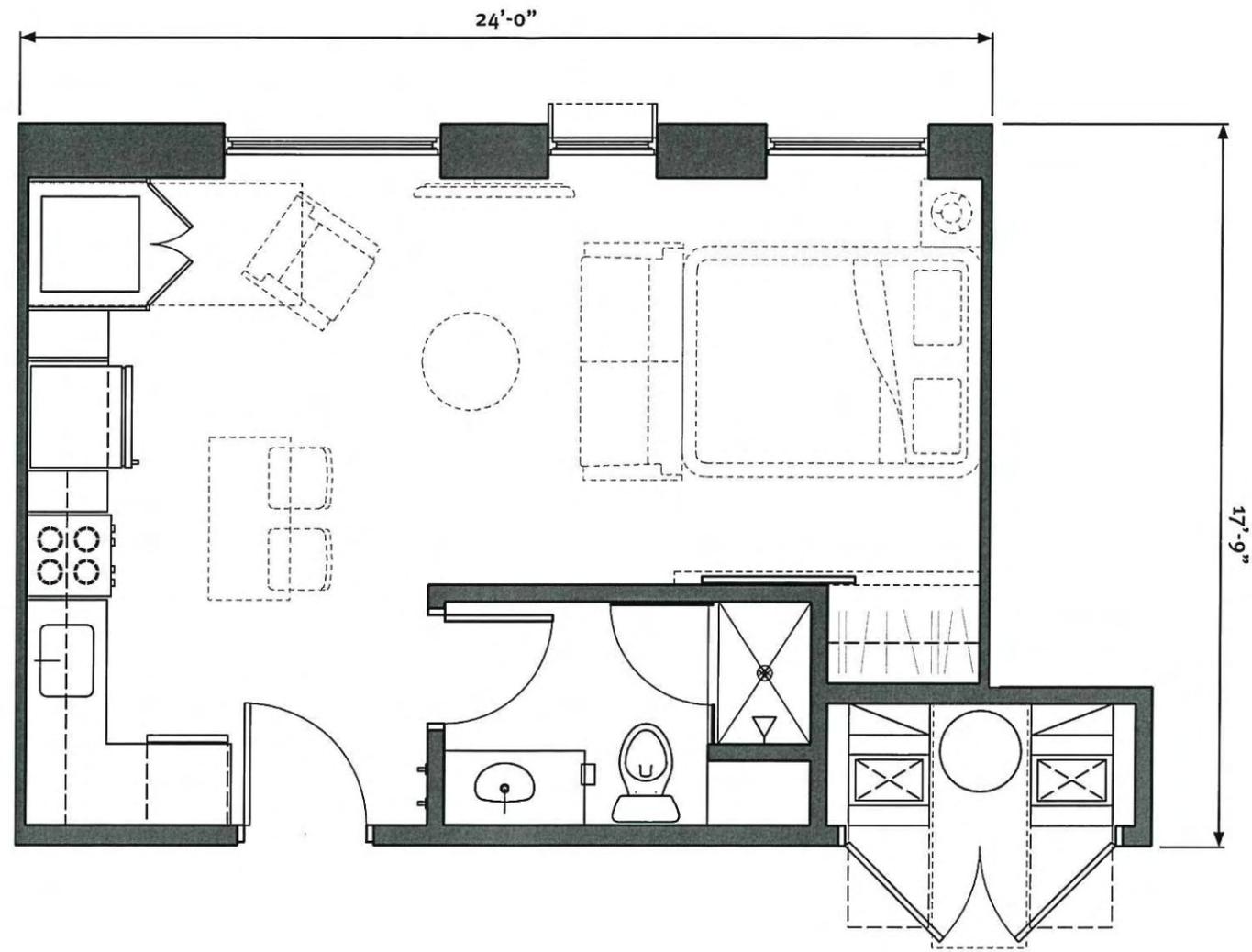
WEST MAIN

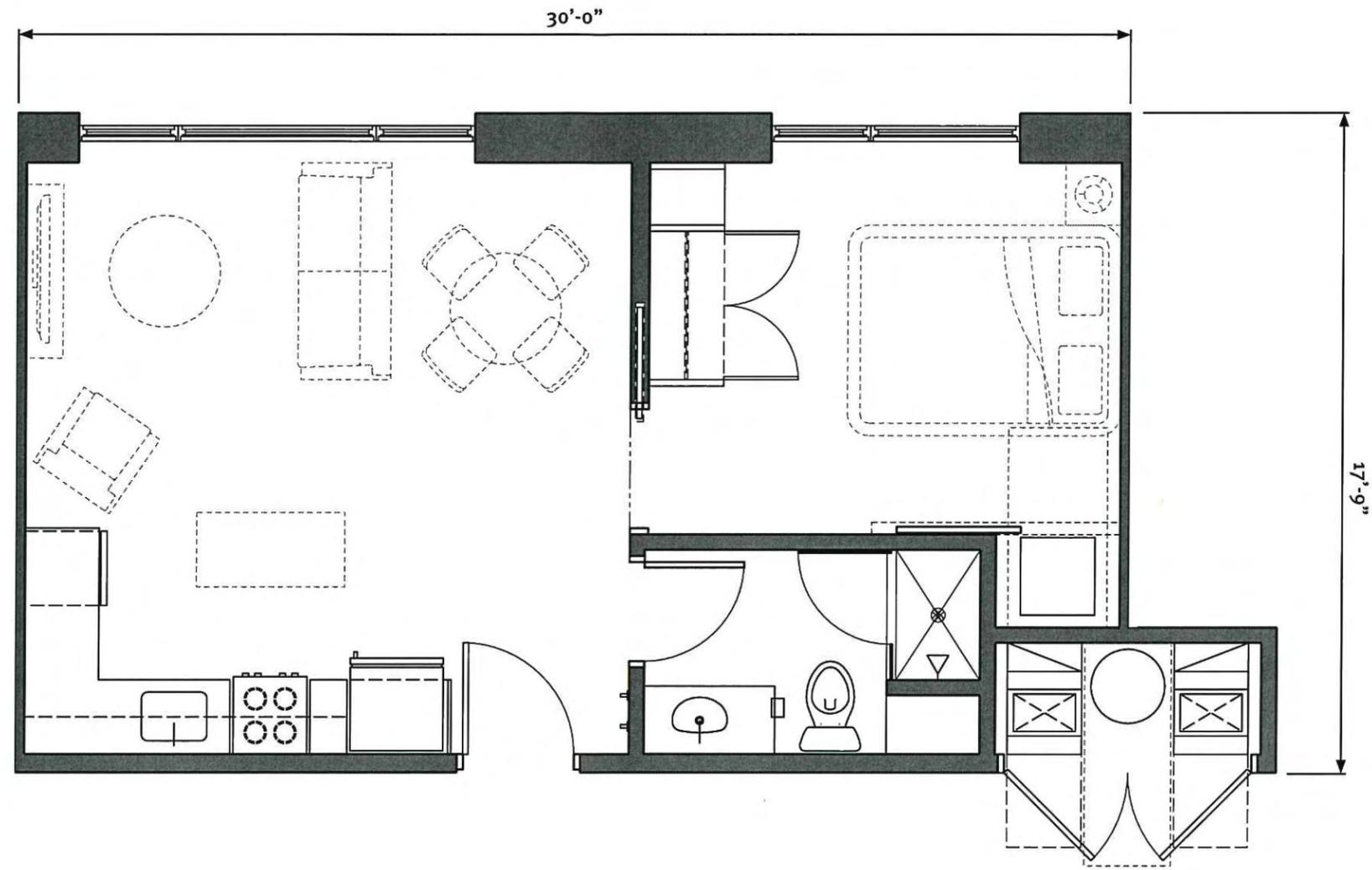


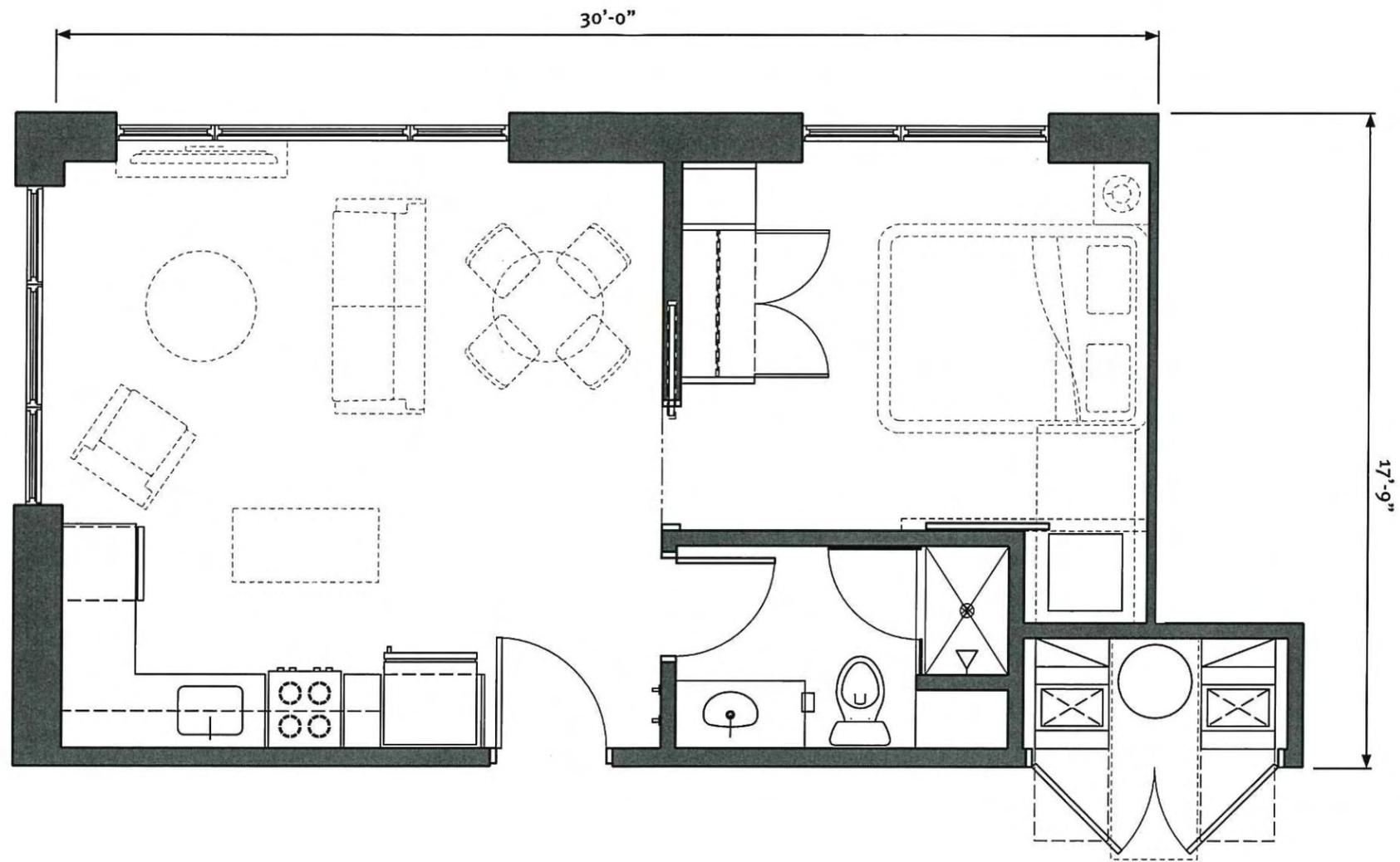
LAKE STREET

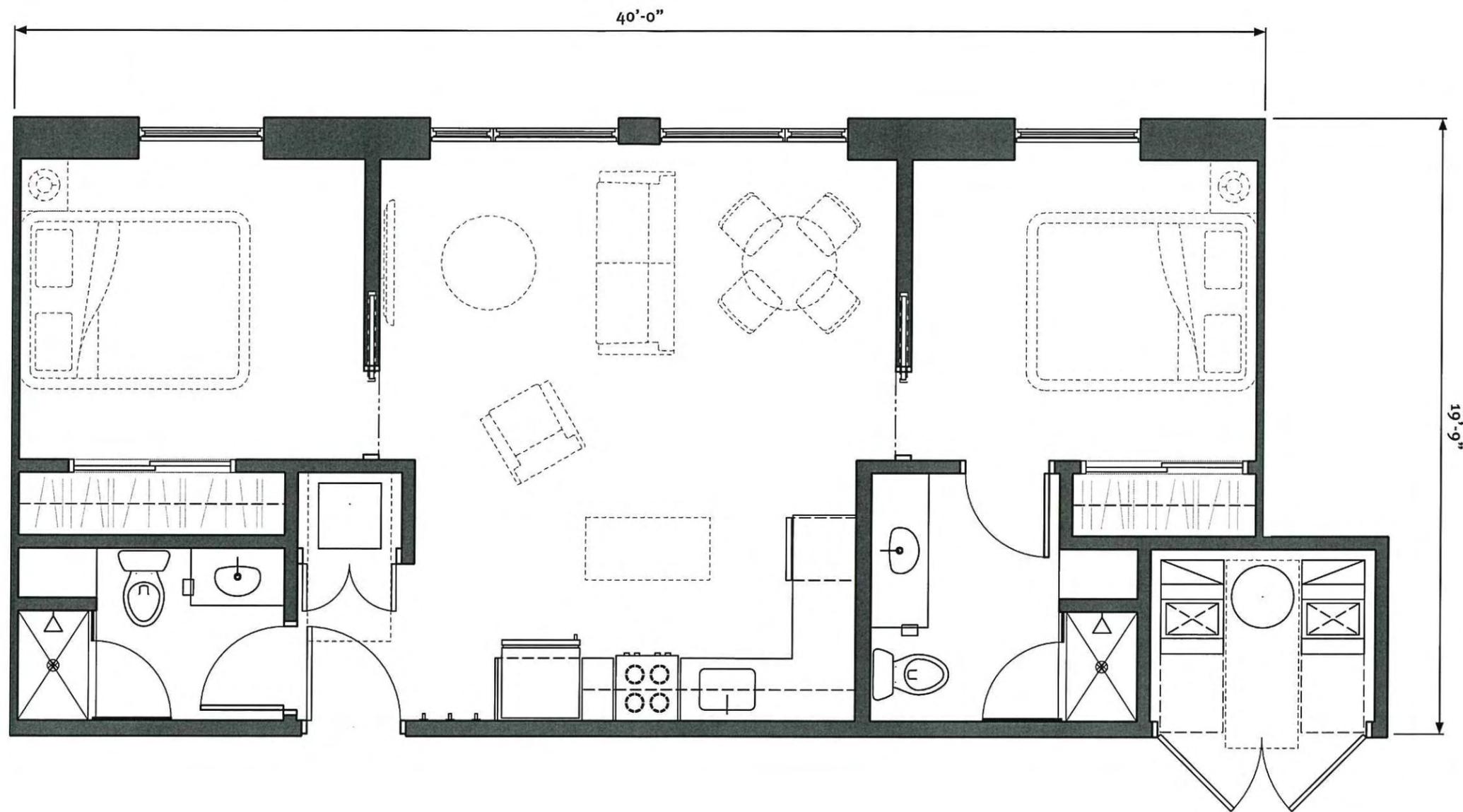


<p><b>CITY OF BOYNE PARKING STUDY</b></p> <p>Boyne City, Michigan</p>	<p><b>RICH &amp; ASSOCIATES PARKING CONSULTANTS</b></p> <p>20077 Northwood Drive, Suite 208 Southfield, MI 48034 Southfield, MI 48034 248.351.5200 Fax: 248.351.2920</p> <p>ARCHITECTS - ENGINEERS - PLANNERS</p>	<p><b>LEGEND:</b></p> <ul style="list-style-type: none"> <li><span style="color: red;">█</span> PRIVATE / RESERVED LOTS</li> <li><span style="color: blue;">█</span> PUBLIC / RESERVED LOTS</li> <li><span style="color: green;">█</span> PUBLIC ON-STREET (UNLIMITED)</li> <li><span style="color: pink;">█</span> 2 HOUR - ON STREET</li> <li><span style="color: orange;">█</span> 15 MINUTE - ON STREET</li> <li><span style="color: blue;">♿</span> BARRIER FREE</li> </ul> <p><b>BLOCK FACE KEY PLAN:</b></p>
	<p>09-14-18 sar</p>	

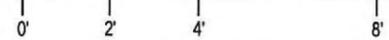


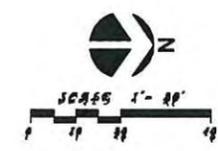






scale

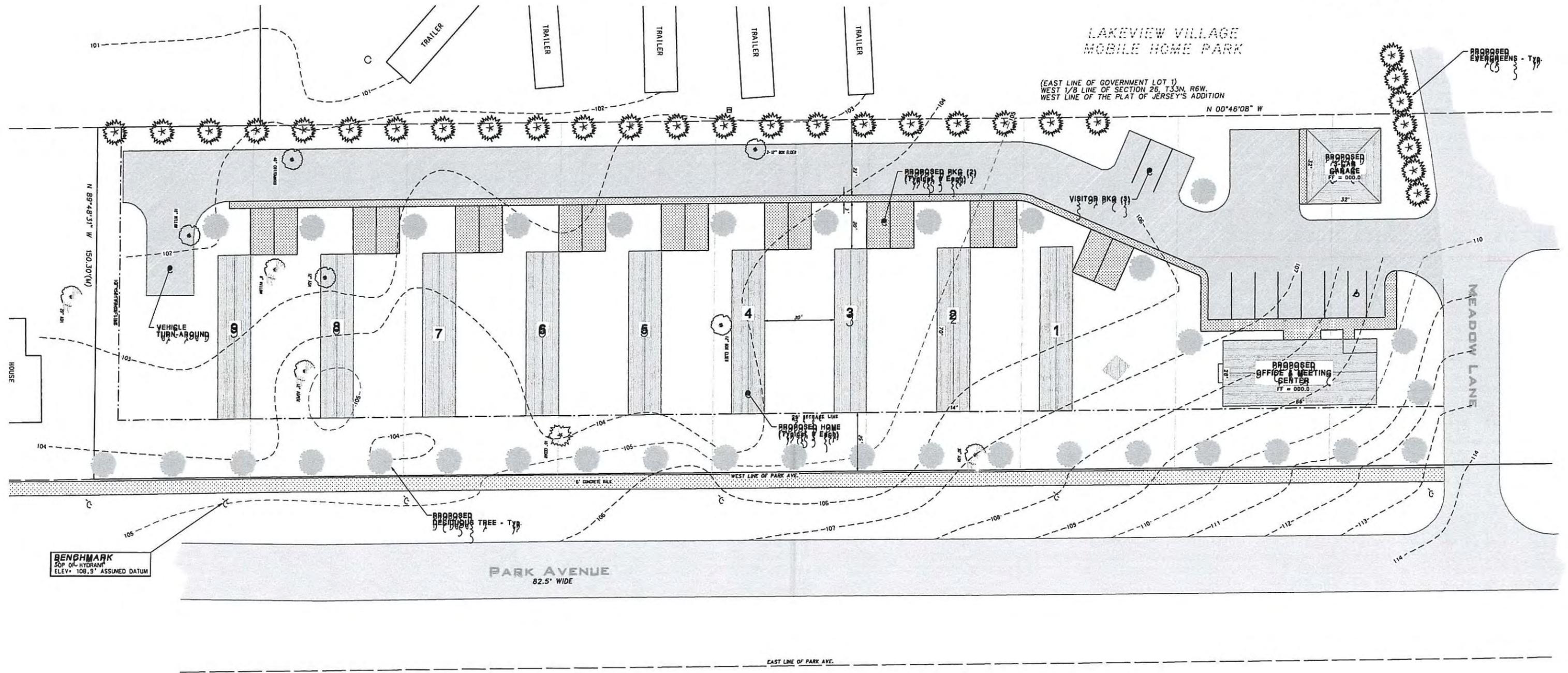




# PRELIMINARY SITE PLAN

## Lakeview Village Mobile Home Park

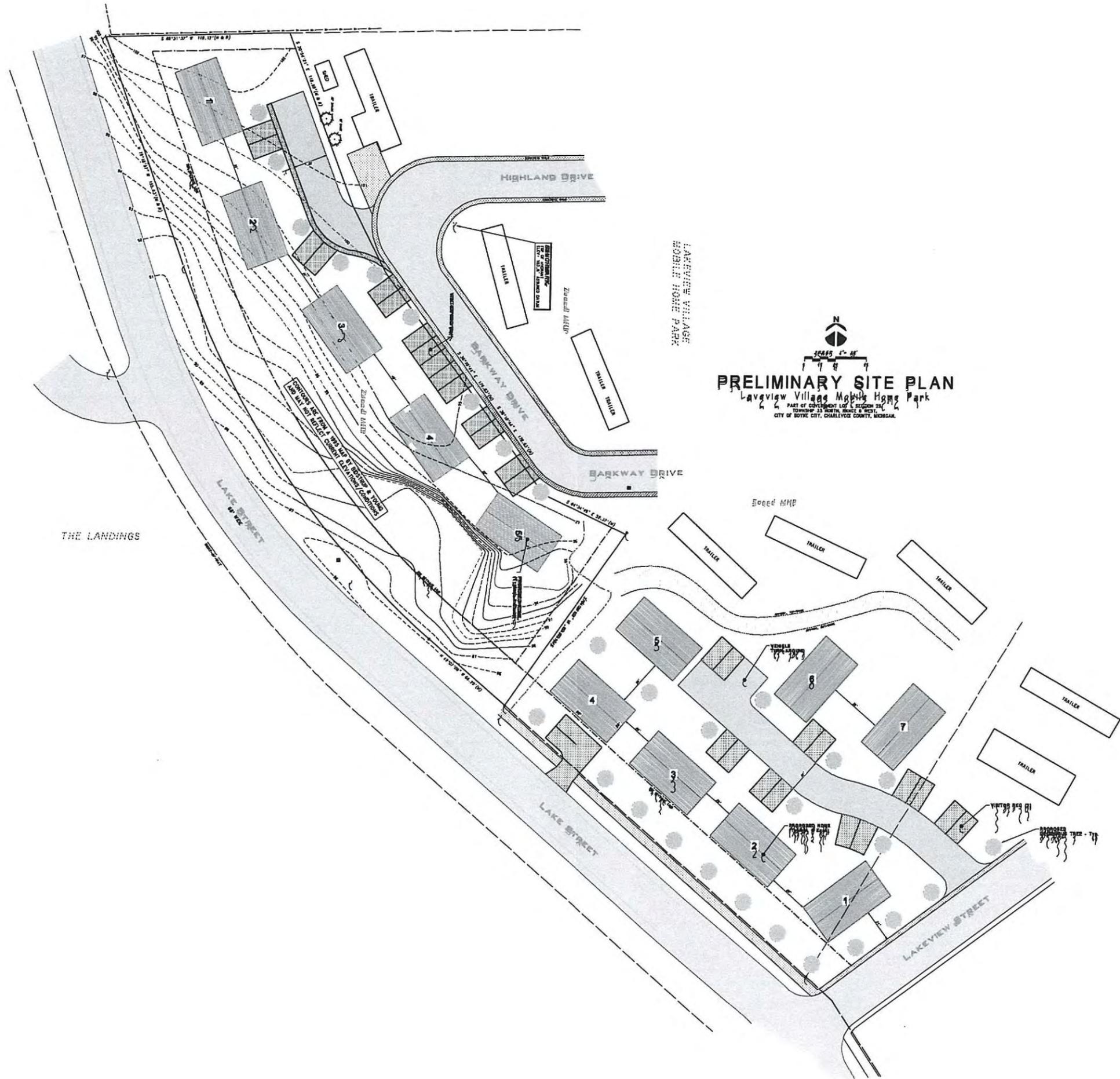
PART OF ASSESSOR'S PLAT OF JERSEY'S ADDITION TO BOYNE CITY,  
SECTION 26, TOWNSHIP 33 NORTH, RANGE 6 WEST,  
CITY OF BOYNE CITY, CHARLEVOIX COUNTY, MICHIGAN.



**BENCHMARK**  
TOP OF HYDRANT  
ELEV. 108.9' ASSUMED DATUM

**PARK AVENUE**  
82.5' WIDE

EAST LINE OF PARK AVE.



  
 1" = 20'  
**PRELIMINARY SITE PLAN**  
 Lakeview Village Mobile Home Park  
 PART OF GOVERNMENT LOT 1, SECTION 24,  
 TOWNSHIP 23 NORTH, RANGE 18 WEST,  
 CITY OF BOYCE CITY, CHARLEVILLE COUNTY, MISSISSIPPI

THE LANDINGS

LAKEVIEW VILLAGE MOBILE HOME PARK

Speed MPH

WATER BODIES  
 PROPOSED TREE - 10'